



PROJECT HIGHLIGHTS:
-MANSION: 6 UNITS
-CARRIAGE HOUSE: 2 UNITS



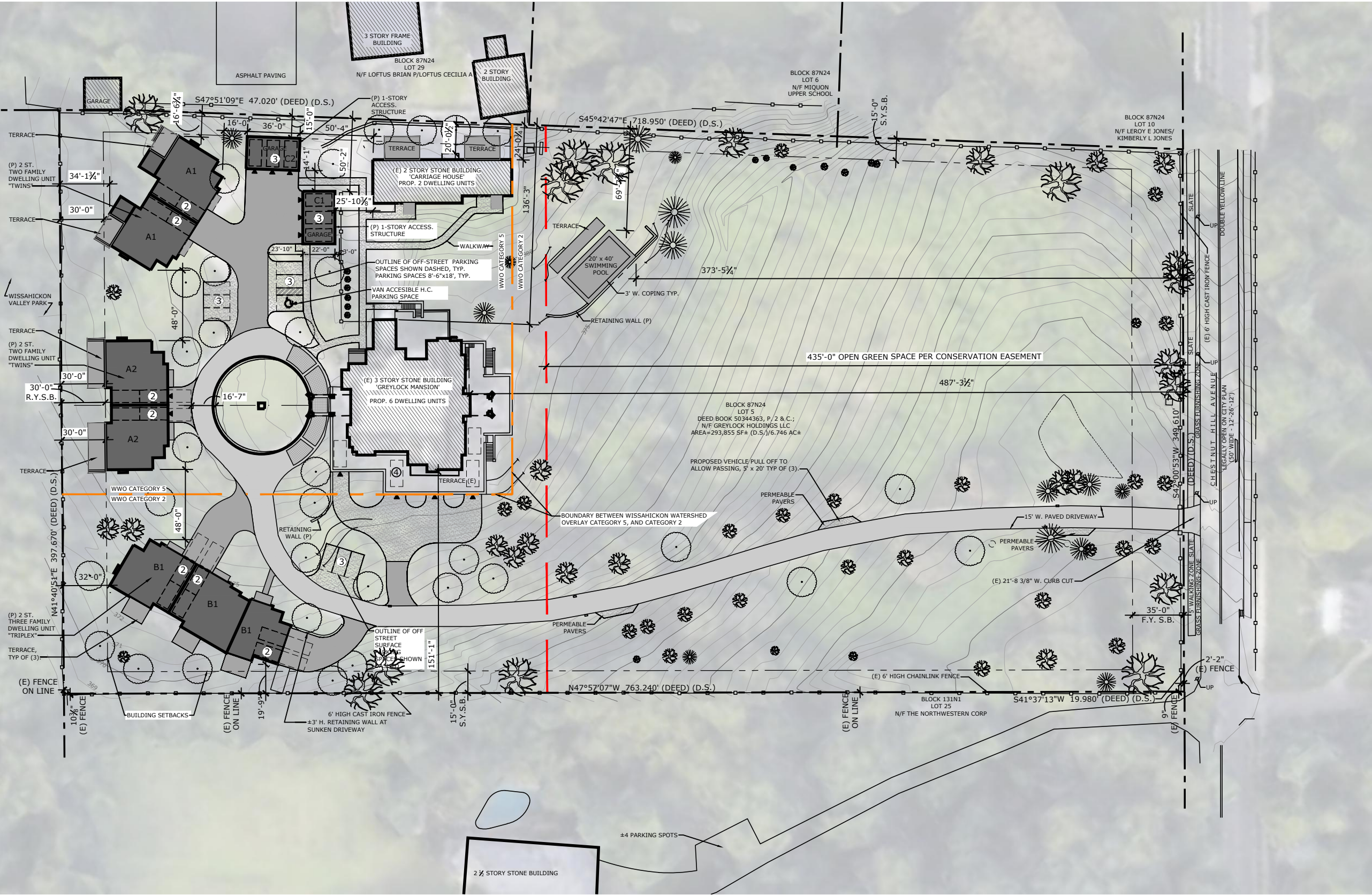
SITE PLAN LEGEND	
	EXTENTS OF EXISTING PAVING
	EXTENTS OF PAVING DEMOLITION
	EXTENTS OF PROPOSED PAVING
	EXTENTS OF PROPOSED PERMEABLE PAVING

TREE SYMBOL LEGEND	
	CONIFEROUS TREE (EXISTING)
	DECIDUOUS TREE (EXISTING)
	CONIFEROUS TREE (TO BE REMOVED)
	DECIDUOUS TREE (TO BE REMOVED)
	PROPOSED TREE





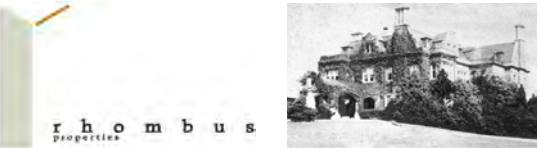
- PROJECT HIGHLIGHTS:
- 15 DWELLING UNITS TOTAL
 - 40 PARKING SPACES TOTAL
 - MANSION: 5 UNITS
 - CARRIAGE HOUSE: 2 UNITS
 - A1, A2: 2 UNITS EACH
 - B: 3 UNITS



SITE PLAN LEGEND	
	EXTENTS OF EXISTING PAVING
	EXTENTS OF PAVING DEMOLITION
	EXTENTS OF PROPOSED PAVING
	EXTENTS OF PROPOSED PERMEABLE PAVING

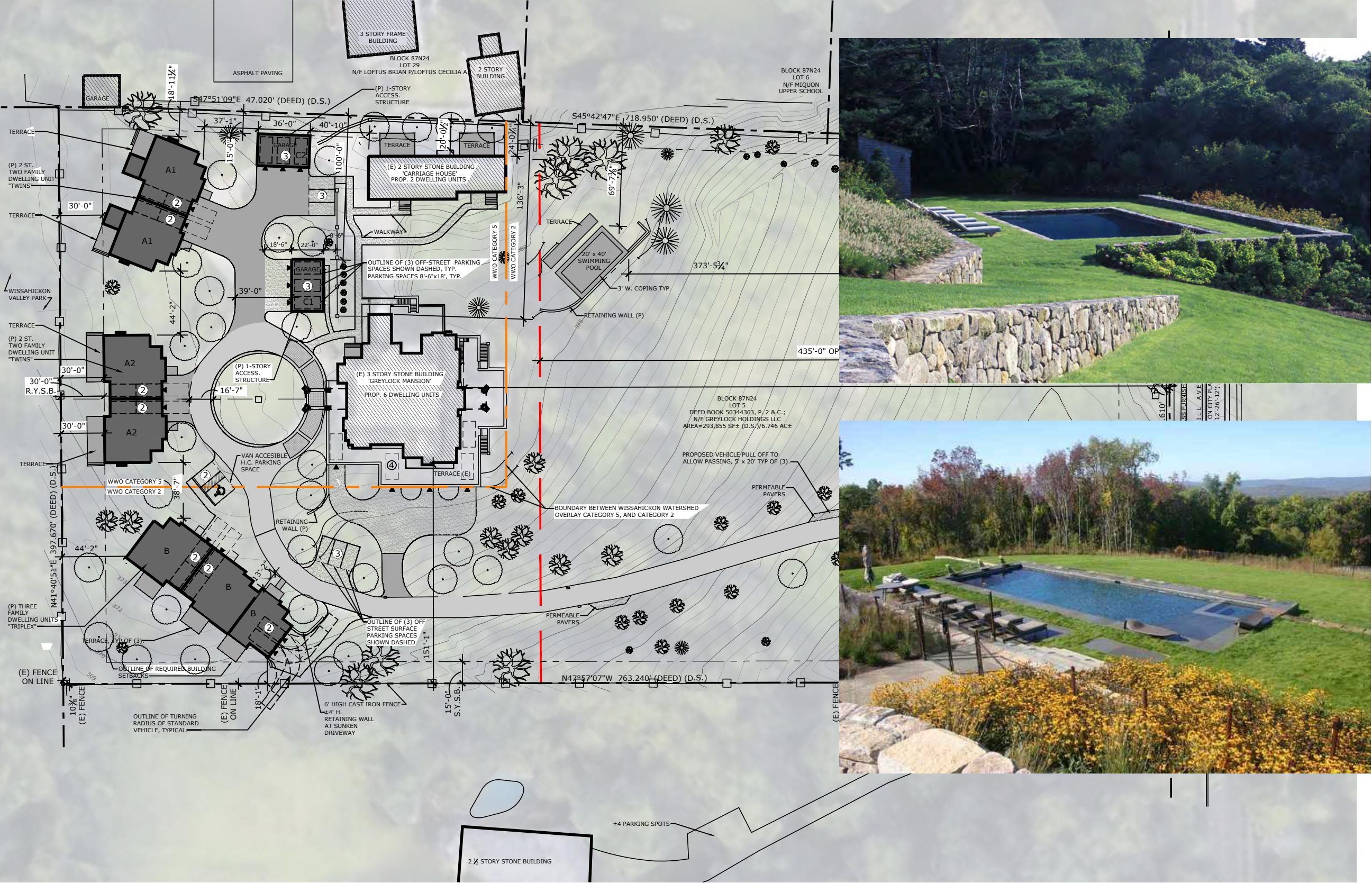
TREE SYMBOL LEGEND	
	CONIFEROUS TREE (EXISTING)
	DECIDUOUS TREE (EXISTING)
	CONIFEROUS TREE (TO BE REMOVED)
	DECIDUOUS TREE (TO BE REMOVED)
	PROPOSED TREE





PROJECT HIGHLIGHTS:

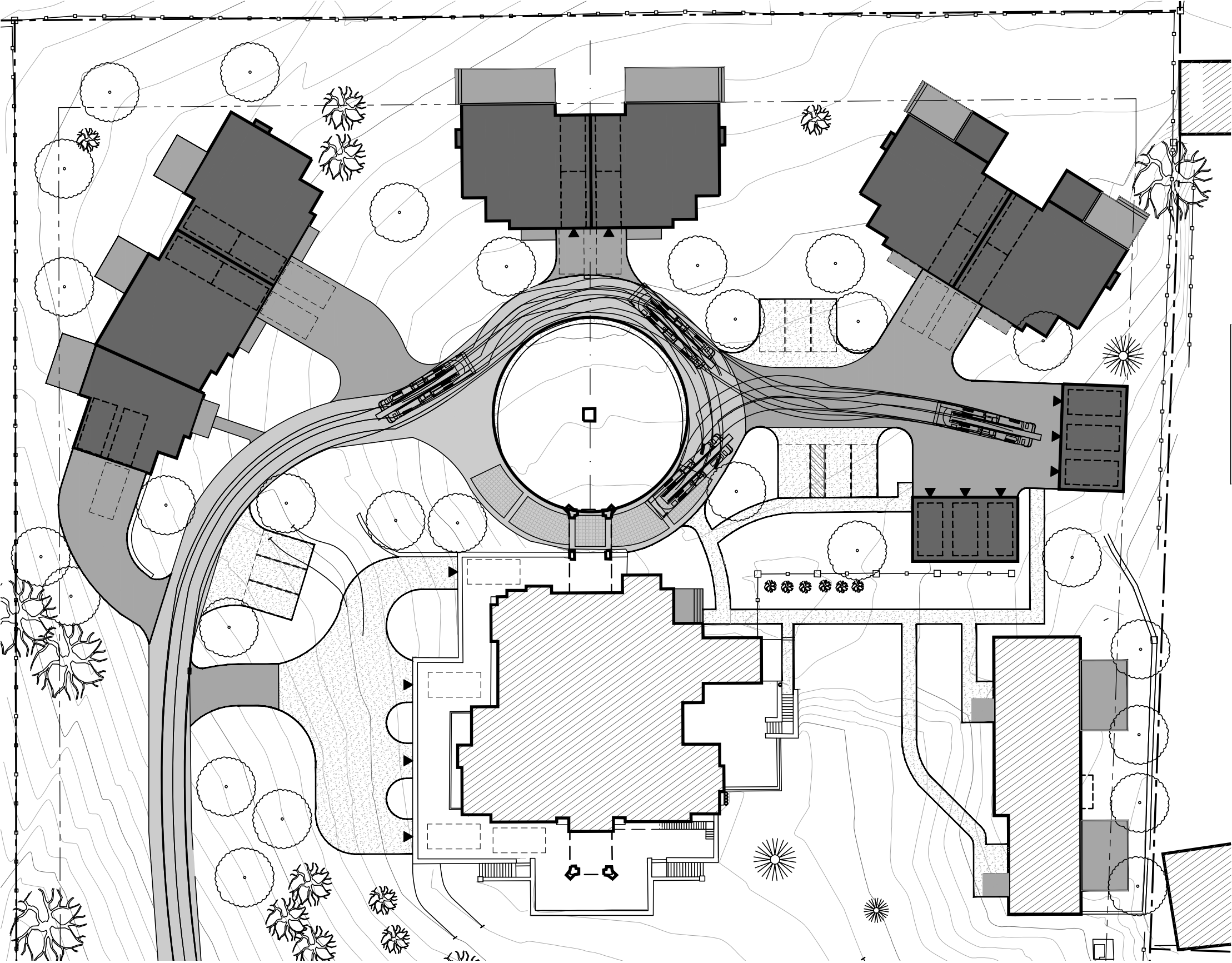
- 15 DWELLING UNITS TOTAL
- 39 PARKING SPACES TOTAL
- MANSION: 6 UNITS
- CARRIAGE HOUSE: 2 UNITS
- A1, A2: 2 UNITS EACH
- B: 3 UNITS



SITE PLAN LEGEND	
	EXTENTS OF EXISTING PAVING
	EXTENTS OF PAVING DEMOLITION
	EXTENTS OF PROPOSED PAVING
	EXTENTS OF PROPOSED PERMEABLE PAVING

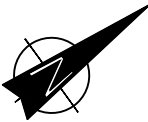
TREE SYMBOL LEGEND	
	CONIFEROUS TREE (EXISTING)
	DECIDUOUS TREE (EXISTING)
	CONIFEROUS TREE (TO BE REMOVED)
	DECIDUOUS TREE (TO BE REMOVED)
	PROPOSED TREE

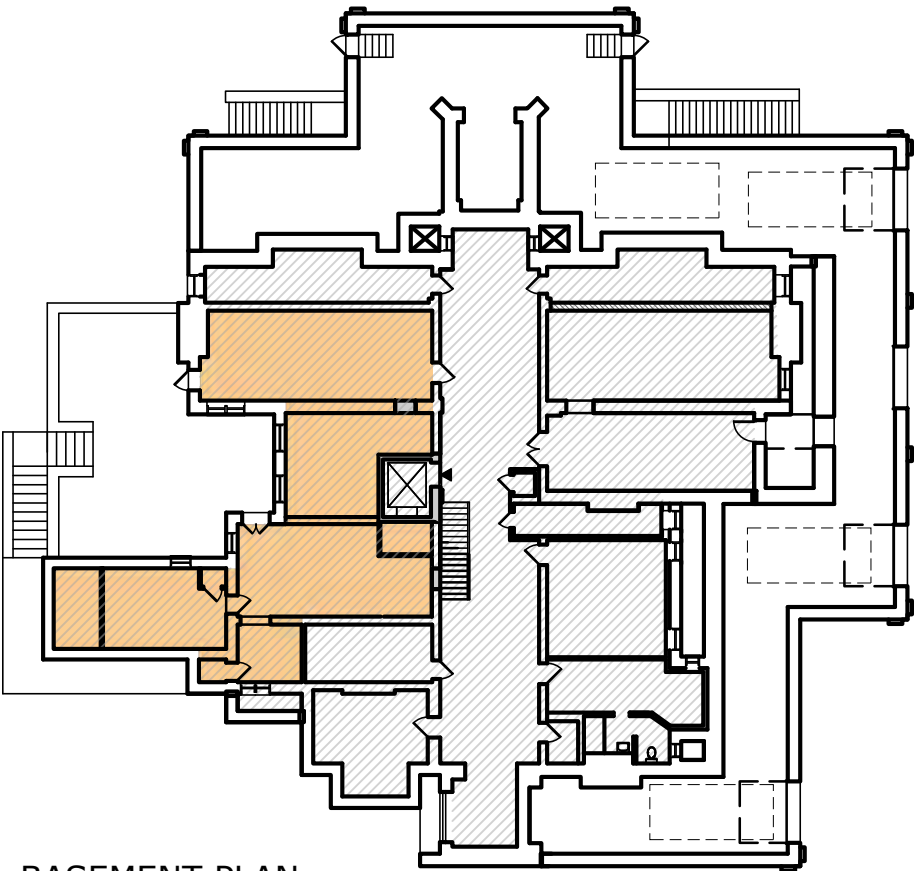




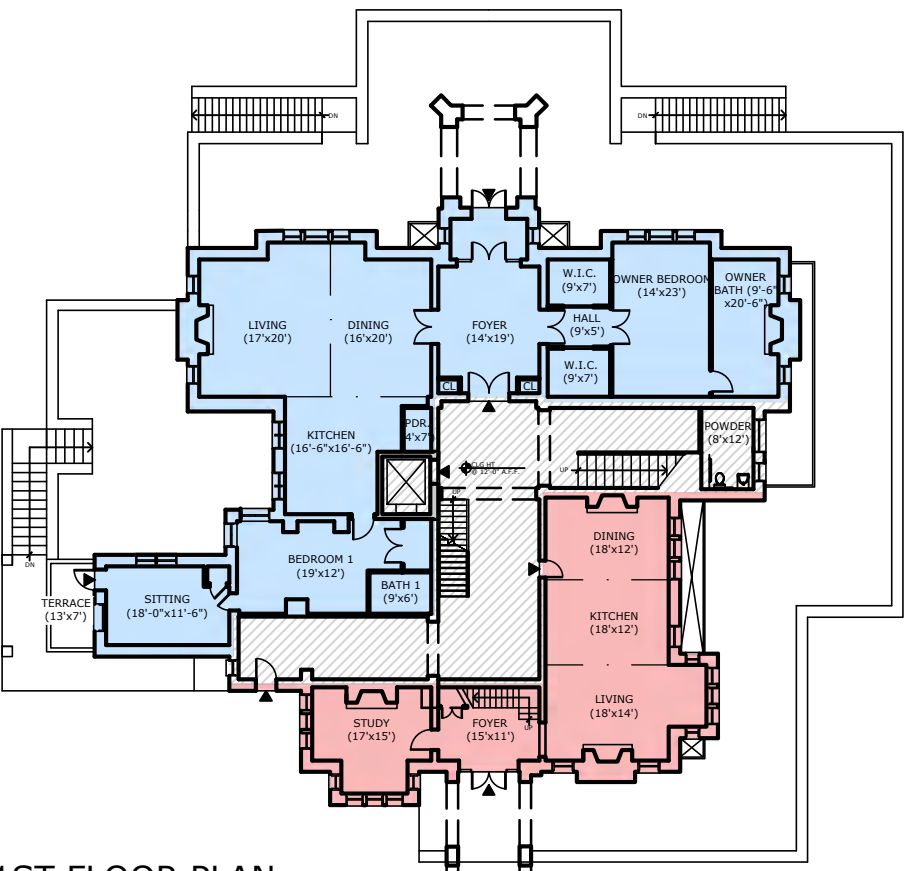
PARKING CHART		Greylock Estate - 209 W. Chestnut Hill Avenue	
BUILDING/SURFACE:	REQ'D # OF SPACES: (1/UNIT)	(P) # OF SPACES:	
A1 (P) 2-ST. TWO FAMILY DWELLING UNIT	2		4
A2 (P) 2-ST. TWO FAMILY DWELLING UNIT	2		4
B (P) 2-ST. THREE FAMILY DWELLING UNIT	3		6
(E) "CARRIAGE HOUSE" - PROP. 2 DWELLING UNITS	2		0
(E) "GREYLOCK MANSION" - PROP. 6 DWELLING UNITS	6		4
C1 - GARAGE (P) 1-STORY ACCESS. STRUCTURE	N/A		3
C2 - GARAGE (P) 1-STORY ACCESS. STRUCTURE	N/A		3
SURFACE PARKING	N/A		15
SURFACE PARKING (VAN ACCESSIBLE H.C. PARKING)	1		1
TOTAL:	16		40
NOTE: ALL OFF-STREET PARKING SPACES ARE 8'-6" x 18'. N/A INDICATES NOT APPLICABLE, (P) INDICATES PROPOSED, (E) INDICATES EXISTING, REQ'D INDICATES REQUIRED.			

ZONING PLAN HATCH LEGEND	
	EXTENTS OF EXISTING PAVING TO REMAIN
	EXTENTS OF PROPOSED PAVING
	EXTENTS OF PROPOSED PERMEABLE PAVING
	EXISTING CONSTRUCTION
	PROPOSED CONSTRUCTION





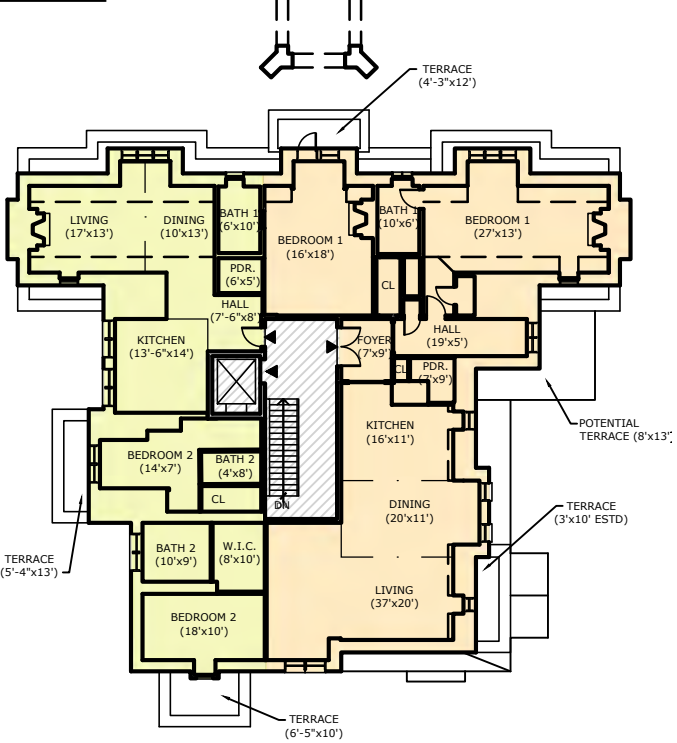
BASEMENT PLAN



1ST FLOOR PLAN



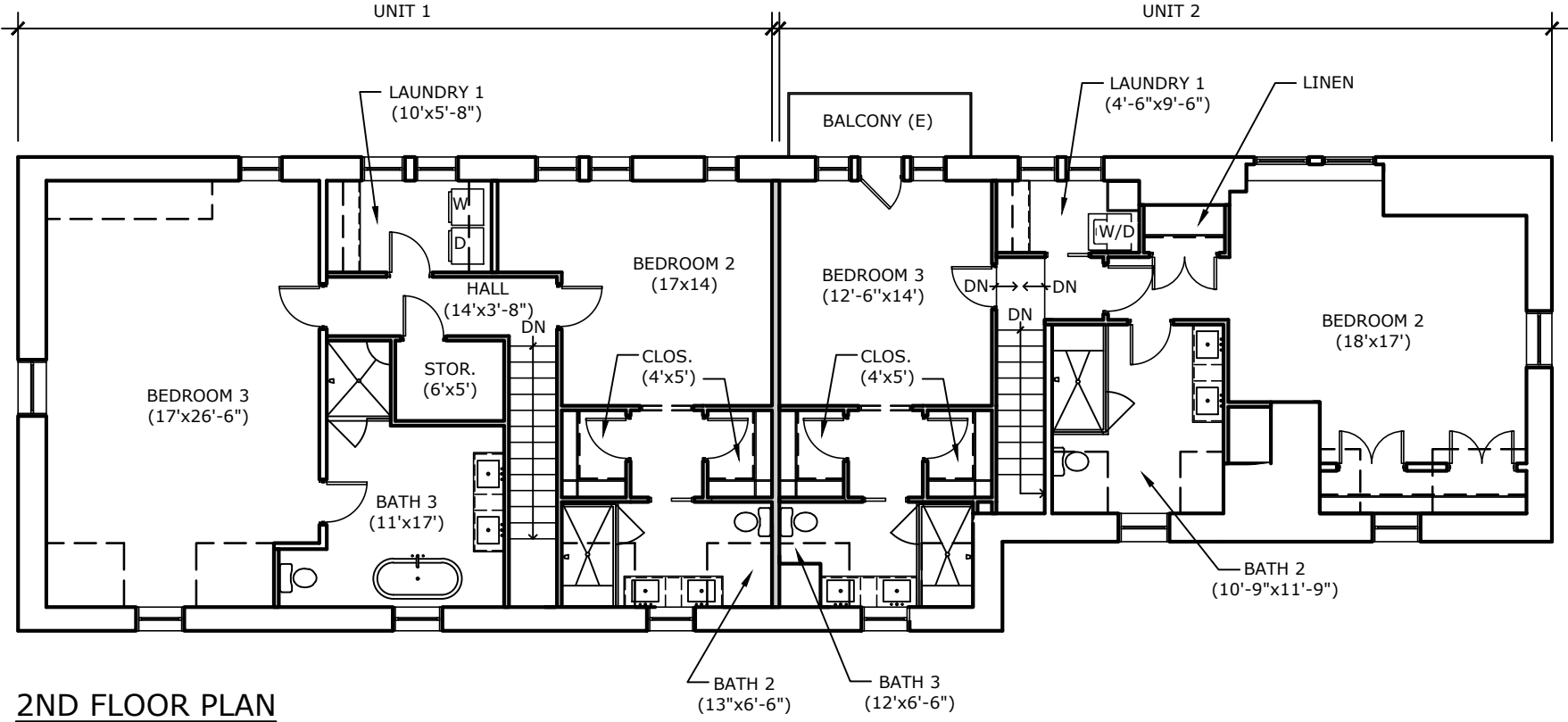
2ND FLOOR PLAN



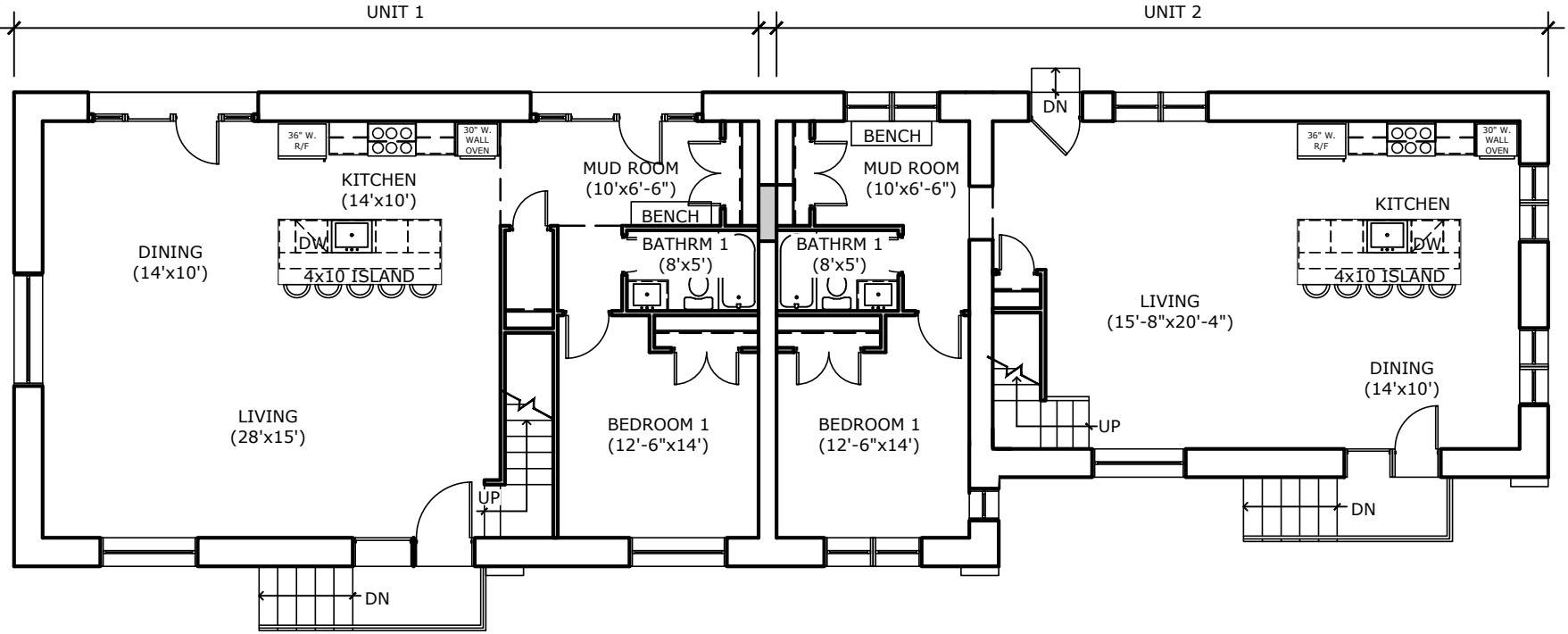
3RD FLOOR PLAN

LEGEND	
	UNIT 1
	UNIT 2
	UNIT 3
	UNIT 4
	UNIT 5
	UNIT 6
	COMMON SPACE



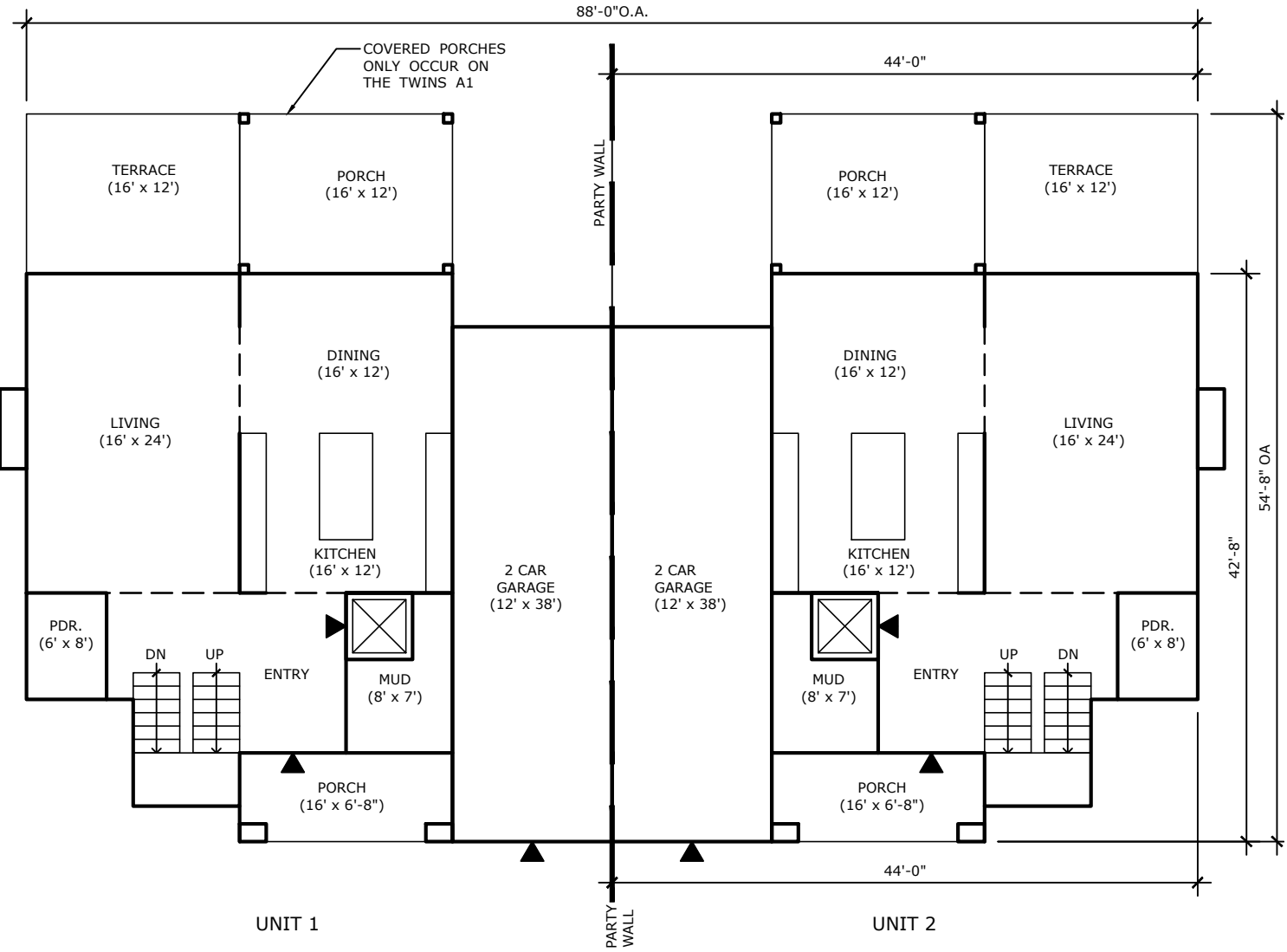


2ND FLOOR PLAN

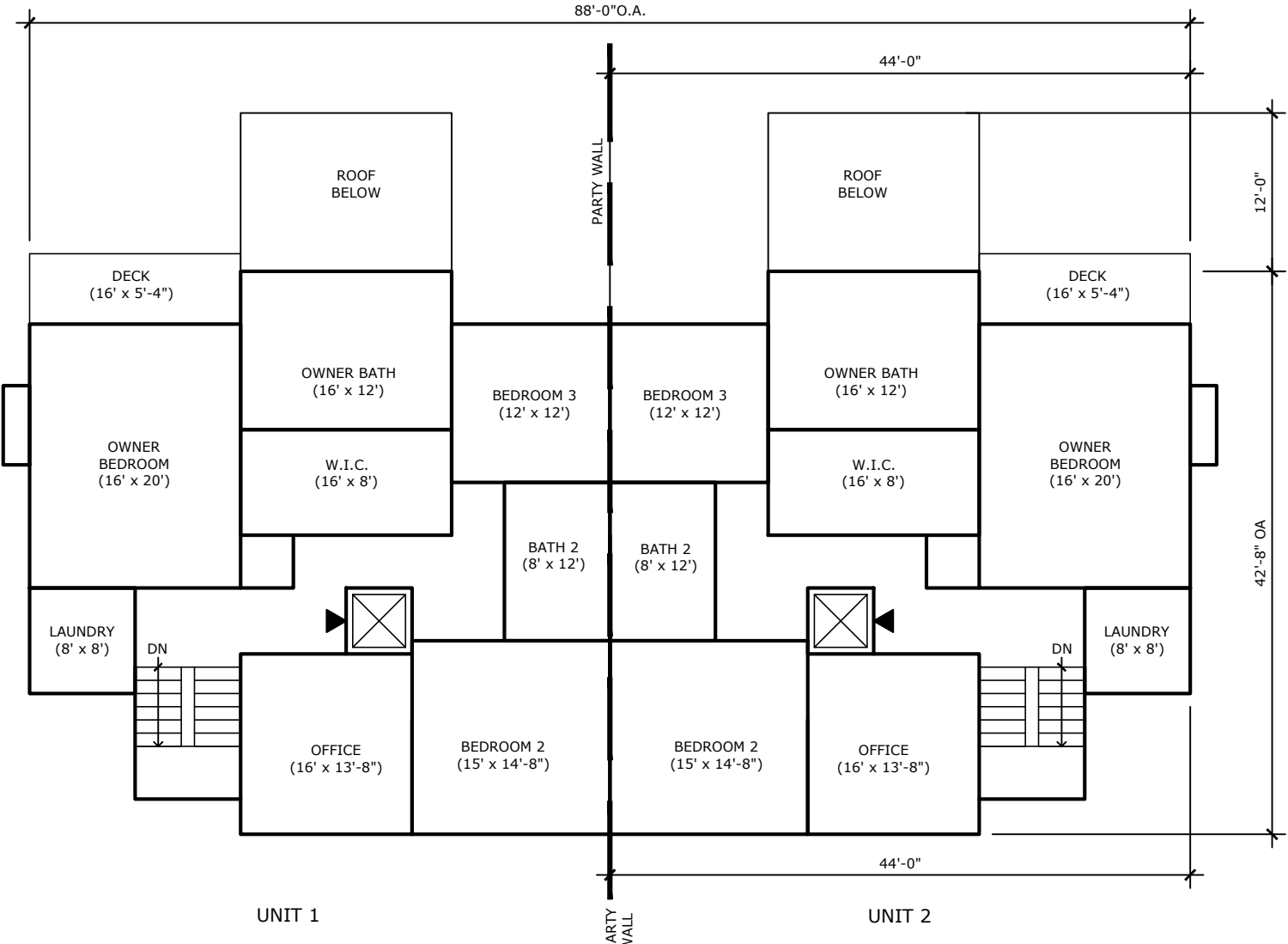


1ST FLOOR PLAN



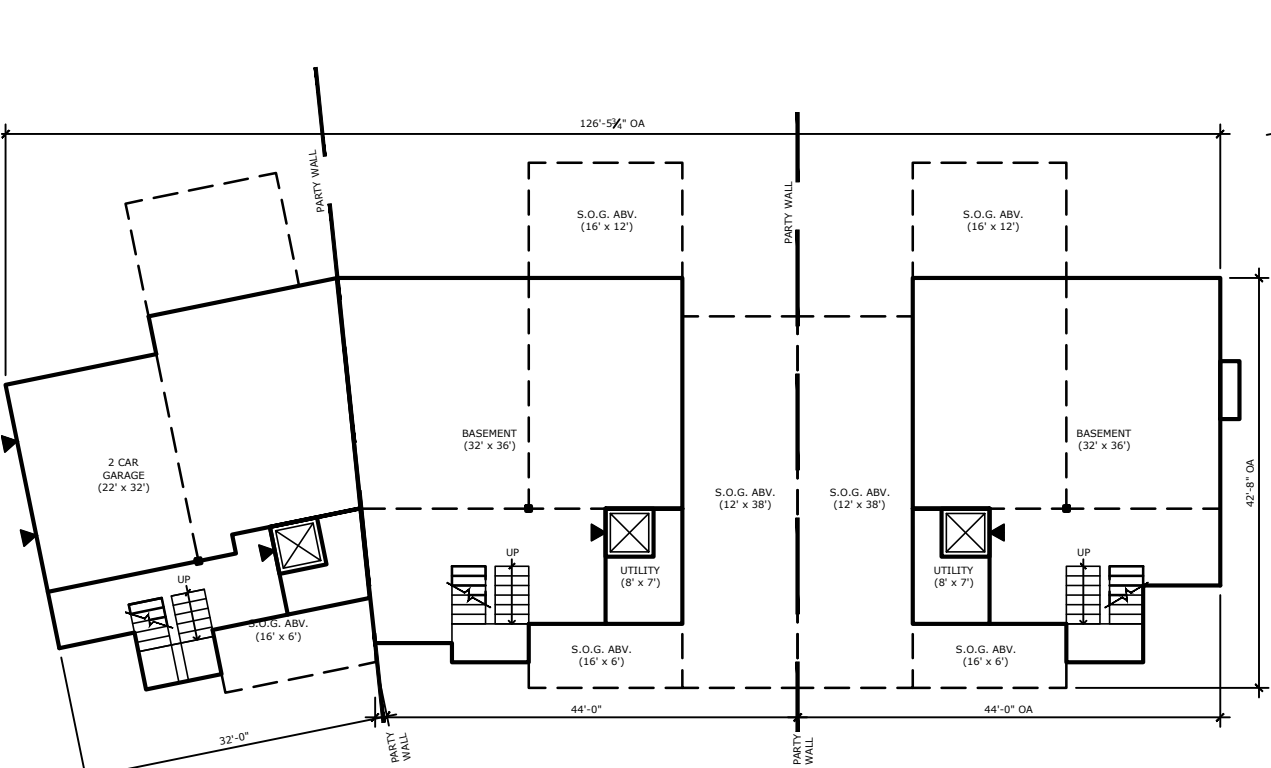


FIRST FLOOR PLAN

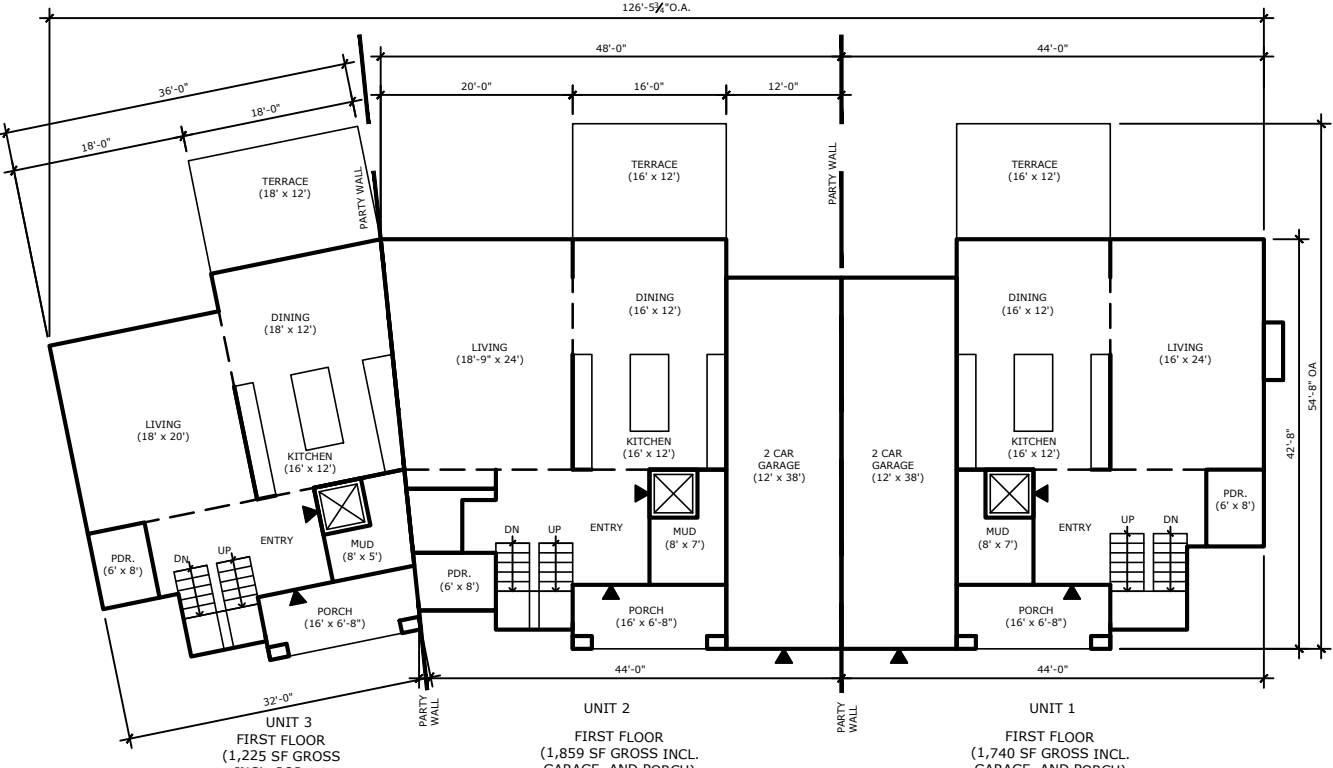


SECOND FLOOR PLAN

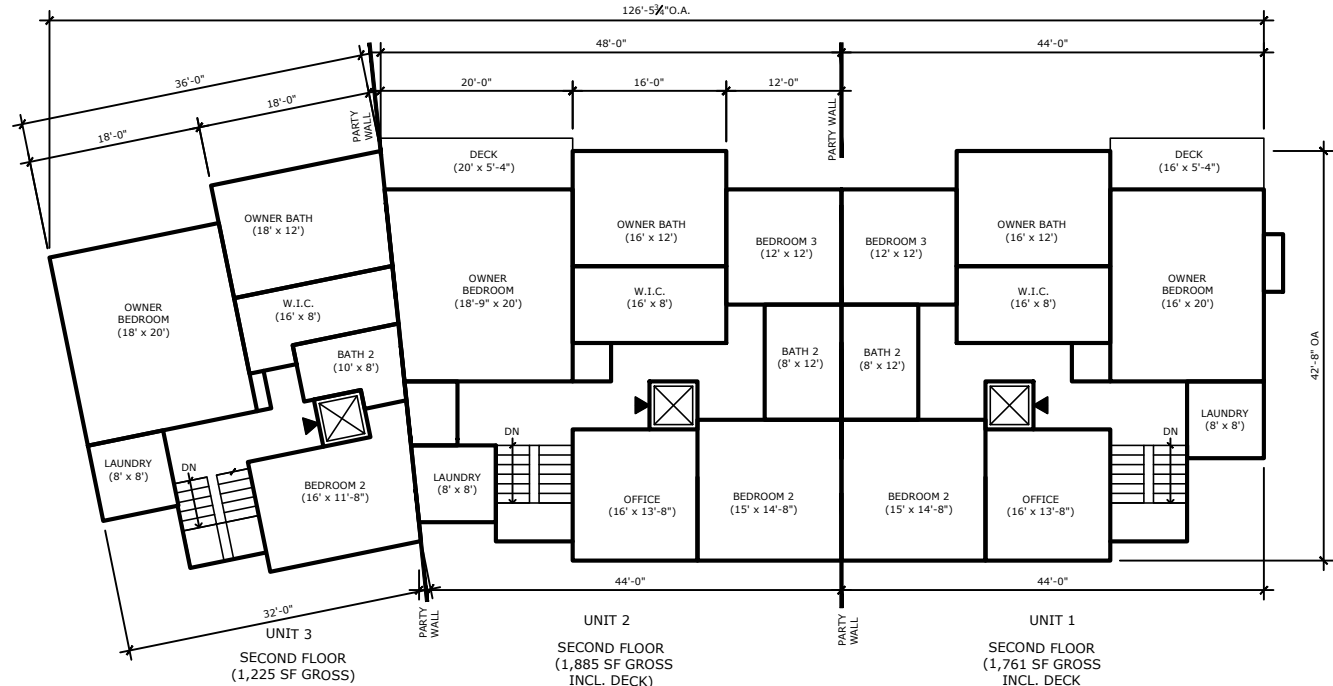




BASEMENT FLOOR PLAN



1ST FLOOR PLAN



2ND FLOOR PLAN





ZONING CODE INFORMATION			Greylock Estate - 209 W. Chestnut Hill Avenue						
ZONING DISTRICT:		RSD-1							
		REQUIRED:		EXISTING:			PROPOSED:		
MIN. LOT WIDTH (FT.):		75.00 FT.		349.61 FT.			349.61 FT.	-	
MIN. LOT AREA (S.F.):		10,000.00 S.F.		295,315.02 S.F.			295,315.02 S.F.		
MAX. OCCUPIED AREA (% OF LOT)		35.0% (MAX.)		9,125.00 S.F.	3.1%		22,528.00 S.F.	7.6%	
MIN. FRONT YARD SETBACK (FT.)		35.00 FT.		487.29 FT.	±		358.85 FT.		
MIN. SIDE YARD SETBACK (FT.)		15.00 FT. (2)		153.7 FT.	±		15/18.08 FT.	(2)	
MIN. REAR YARD SETBACK (FT.)		30.00 FT.		165 FT.	±		30 FT.		
MAX. BUILDING HEIGHT (FT.)		38.00 FT. (MAX.)		44.17 FT.	± **		44.17 FT.	± **	
* PROPERTY LOCATED WITHIN WISSAHICKON WATERSHED OVERLAY DISTRICT - 79,672 S.F. WITHIN CATEGORY 5 - NO LIMIT - 215,643 S.F. WITHIN CATEGORY 2 - 27% MAX. IMPERVIOUS COVERAGE.									
** DENOTES EXISTING NON CONFORMING CONDITION.		AT FLAT ROOFS BLDG. HT. IS MEASURED TO T.O. STRUCTURE							
*** DENOTES PROPOSED NON CONFORMING CONDITION.									

Property Location:		209 W Chestnut Hill Ave					
Zoning District:		RSD-1					
Impervious Coverage Calculations for Total Lot Area							3/7/2024
Area	Existing	%	Total Proposed	%	Net Increase/ Decrease	Notes	
Total Lot Area	295,315.00 sqft.		295,315.00 sqft.		N/A		
Buildings	9,125.00 sqft.	4.23%	22,528.00 sqft.	7.63%	13,403.00		
Driveway Paving	24,176.00 sqft.	11.21%	21,471.30 sqft.	7.27%	(2,704.70)		
Terraces, Walls & Walkways	7,610.90 sqft.	3.53%	18,225.12 sqft.	6.17%	10,614.22	Includes 5,878 sqft. of permeable paving	
Pool & Pool Terrace	- sqft.	0.00%	1,643.00 sqft.	0.56%	1,643.00		
Total Area	40,911.90 sqft.	18.97%	63,867.42 sqft.	21.63%	22,955.52		
Property located within Wissahickon Watershed Overlay District - 79,672 sqft. Located within Category 5 - No Limit; 215,643 sqft. Located within Category 2 - 27% Max. Impervious Coverage. Impervious coverage calculations provided are for the total area of the lot.							
*Existing impervious coverage % and area calcs are based upon lot area and are based on existing conditions survey dimensions by Maser Engineering							





Property Location: 209 W Chestnut Hill Ave

Zoning District: RSD-1

3/7/2024

Impervious Coverage Calculations for Portion of Lot within WWOD Category 2

Area	Existing (SF)		%	Proposed (SF)		%	Net Increase/ Decrease	Notes
Lot Area in WWOD Category 2	215,643.00	sqft.		215,643.00	sqft.		N/A	27% Maximum Impervious Coverage in Category 2
Buildings	-	sqft.	0.00%	4,824.00	sqft.	2.24%	4,824.00	
Driveway Paving	10,012.00	sqft.	4.64%	12,623.00	sqft.	5.85%	2,611.00	Includes 3464 sqft. of permeable paving
Terraces, Walls & Walkways	32.00	sqft.	0.01%	4,951.12	sqft.	2.30%	4,919.12	
Pool & Pool Terrace	-	sqft.	0.00%	1,643.00	sqft.	0.76%	1,643.00	
Total Area	10,044.00	sqft.	4.66%	24,041.12	sqft.	11.15%	13,997.12	

Property located within Wissahickon Watershed Overlay District - 79,672 sqft. Located within Category 5 - No Limit; 215,643 sqft. Located within Category 2 - 27% Max. Impervious Coverage. Impervious coverage calculations provided are only for the area of the lot located within Category 2.

*Existing impervious coverage % and area calcs are based upon lot area and are based on existing conditions survey dimensions by Maser Engineering

Impervious Coverage Calculations for Portion of Lot within WWOD Category 2

Notice of: ☒ **Refusal** ☐ **Referral**

Application Number: ZP-2023-005003	Zoning District(s): RSD1	Date of Refusal: 9/7/2023
Address/Location: 209 W CHESTNUT HILL AVE, Philadelphia, PA 19118-3703 Parcel (PWD Record)		Page Number Page 1 of 2
Applicant Name: Matthew Millan	Applicant Address: 15 W Highland Avenue Philadelphia, PA 19118 USA	Civic Design Review? N

Application for:

FOR THE ERECTION OF A STRUCTURE (B) FOR USE AS A MULTI-FAMILY HOUSEHOLD LIVING FOR FIVE (5) DWELLING UNITS WITH SEVEN (7) ATTACHED ACCESSORY PRIVATE PARKING GARAGES. FOR THE ERECTION OF TWO (2) STRUCTURES (A1 & A2) FOR USE AS A TWO (2) FAMILY HOUSEHOLD LIVING EACH (TOTAL FOUR DWELLING UNITS); WITH FOUR (4) ATTACHED ACCESSORY PRIVATE PARKING GARAGES IN EACH STRUCTURE. FOR CHANGE IN USE OF AN EXISTING STRUCTURE FOR USE AS A MULTI-FAMILY HOUSEHOLD LIVING FOR FOUR (4) DWELLING UNITS WITH FOUR (4) ATTACHED ACCESSORY PRIVATE PARKING. FOR CHANGE IN USE OF AN EXISTING STRUCTURE FOR USE AS A TWO (2) FAMILY HOUSEHOLD LIVING. FOR INSTALLING OF A SWIMMING POOL AND AN ACCESSORY STRUCTURE. FOR THE ERECTION OF TWO DETACHED ACCESSORY PRIVATE RESIDENCE GARAGES (C1&C2) FOR USE AS THREE (3) PARKING SPACES EACH. FOR NINE (9) ACCESSORY OFF-STREET SURFACE PARKING SPACES. SIZE AND LOCATION AS SHOWN IN APPLICATION / PLAN.

The permit for the above location cannot be issued because the proposal does not comply with the following provisions of the Philadelphia Zoning Code. (Codes can be accessed at www.phila.gov.)

<u>Code Section(s):</u>	<u>Code Section Title(s):</u>	<u>Reason for Refusal:</u>
14-401(4)(b)	Multiple Principal Buildings in Residential Districts	Whereas no more than one principal building is allowed per lot in RSD, zoning districts. While the proposed development has more than one building at the lot
14-704(2)(b)	Steep Slope	Where the slope of land is 25% or greater, no site clearing or earth-moving activity is permitted. The lot has a slope of over 25% in some areas, where development are proposed.
14-705(1)(g)	Tree Replacement	The total caliper inches of all replacement trees shall be no less than the total inches of DBH of all trees removed from the lot. The required tree replacement shall be 957.5 caliper inches but the proposed are 195.0 caliper inches.



ASAD ULLAH KHAN
PLANS EXAMINER

9/7/2023
DATE SIGNED

Notice to Applicant: An appeal from this decision may be made to the Zoning Board of Adjustment, One Parkway Building, 1515 Arch St., 18th Fl., Phila., PA 19102 within thirty (30) days of date of Refusal / Referral. Please see appeal instructions for more information.

Notice of: ☒ **Refusal** ☐ **Referral**

Application Number: ZP-2023-005003	Zoning District(s): RSD1	Date of Refusal: 9/7/2023
Address/Location: 209 W CHESTNUT HILL AVE, Philadelphia, PA 19118-3703 Parcel (PWD Record)		Page Number Page 2 of 2
Applicant Name: Matthew Millan	Applicant Address: 15 W Highland Avenue Philadelphia, PA 19118 USA	Civic Design Review? N

Table 14-701-1	Dimensional Standards for Lower Density Residential Districts	Whereas, the minimum rear yard in RSD-1 zoning district shall be 30 ft, while the proposed lot has a rear yard less of 18 ft.
Table 14-802-4	Required Parking for Persons with Disabilities	For nine (9) parking spaces, one (1) shall be accessible and for every <i>six</i> or a fraction of <i>six</i> accessible parking spaces, at least one shall be a van-accessible parking space complying with ICC/ANSI A117.1. The proposed development has no accessible parking.

ONE (1) USE REFUSAL
Four (4) ZONING REFUSAL

Fee to File Appeal: \$ 300

Note to the ZBA: App # 991208043, cal # 99-1471 , granted a variance for an office use

App # 000922007, cal # 00-1183 granted a variance for an additional office space.

Parcel Owner: GREYLOCK DEVELOPMENT PARTNERS LP



ASAD ULLAH KHAN
PLANS EXAMINER

9/7/2023
DATE SIGNED

Notice to Applicant: An appeal from this decision may be made to the Zoning Board of Adjustment, One Parkway Building, 1515 Arch St., 18th Fl., Phila., PA 19102 within thirty (30) days of date of Refusal / Referral. Please see appeal instructions for more information.



PROJECT HIGHLIGHTS:
-15 DWELLING UNITS TOTAL
-39 PARKING SPACES TOTAL
-MANSION: 6 UNITS
-CARRIAGE HOUSE: 2 UNITS
-A1, A2: 2 UNITS EACH
-B: 3 UNITS



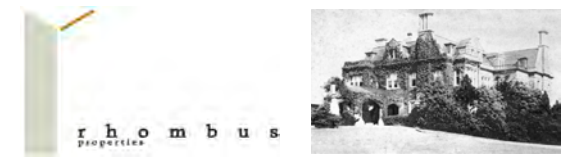
SITE PLAN LEGEND	
	EXTENTS OF EXISTING PAVING
	EXTENTS OF PAVING DEMOLITION
	EXTENTS OF PROPOSED PAVING
	EXTENTS OF PROPOSED PERMEABLE PAVING

TREE SYMBOL LEGEND	
	CONIFEROUS TREE (EXISTING)
	DECIDUOUS TREE (EXISTING)
	CONIFEROUS TREE (TO BE REMOVED)
	DECIDUOUS TREE (TO BE REMOVED)
	PROPOSED TREE





View 1 - From Driveway

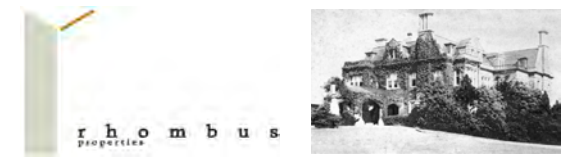


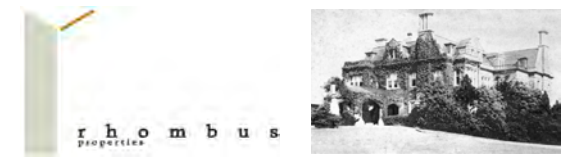
View 1 - From Driveway without Meadow



View 2 - From Driveway







View 3 - From Driveway without Meadow



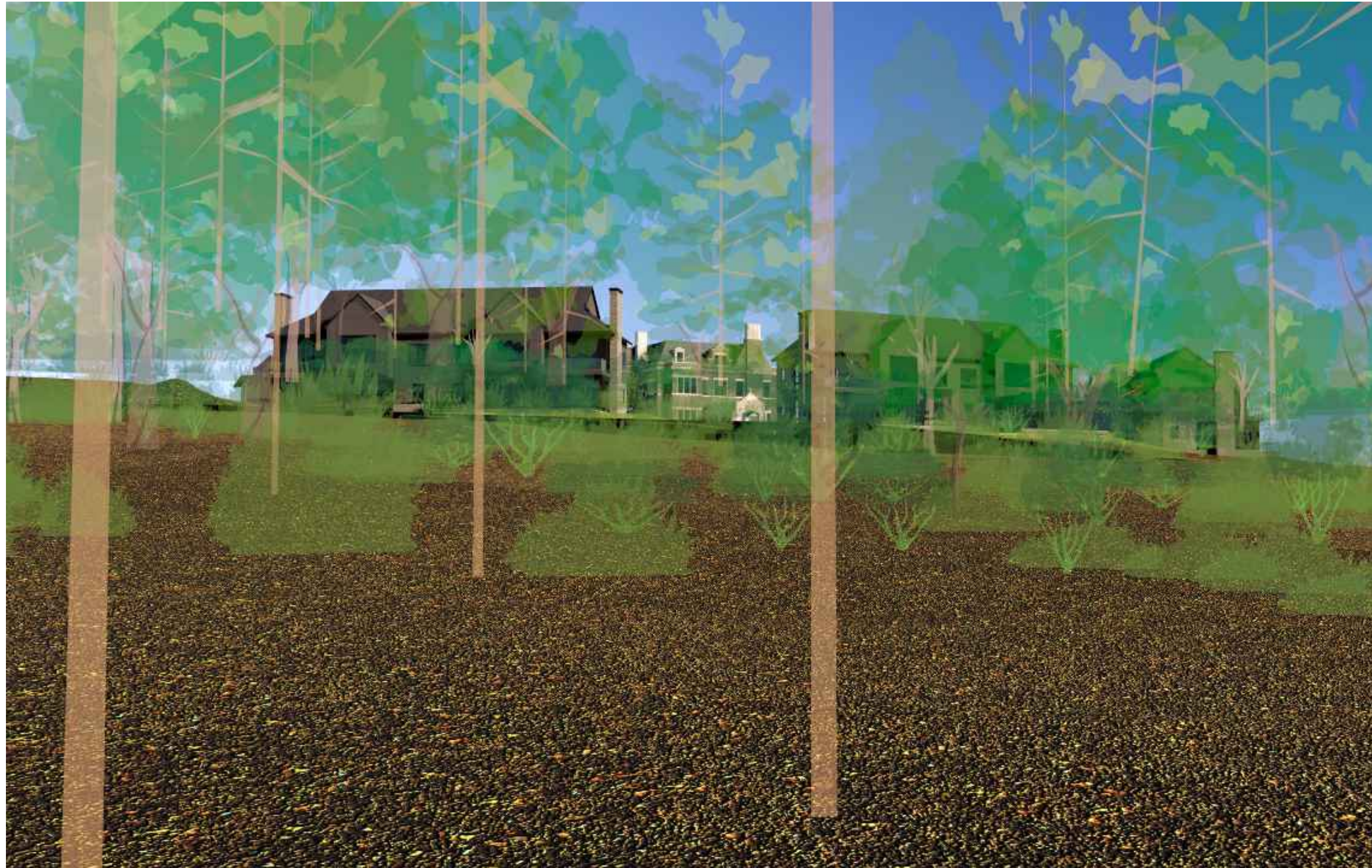
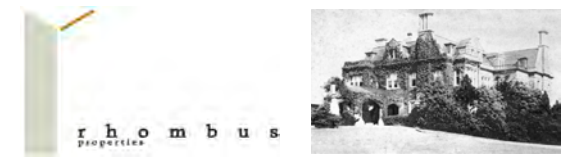


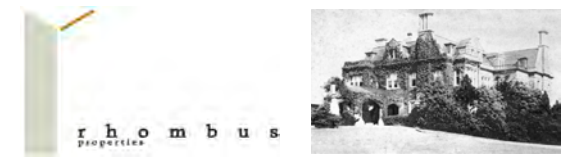
Existing View from Lavender Trail Aligned with North West Property Corner - 11/29/2022





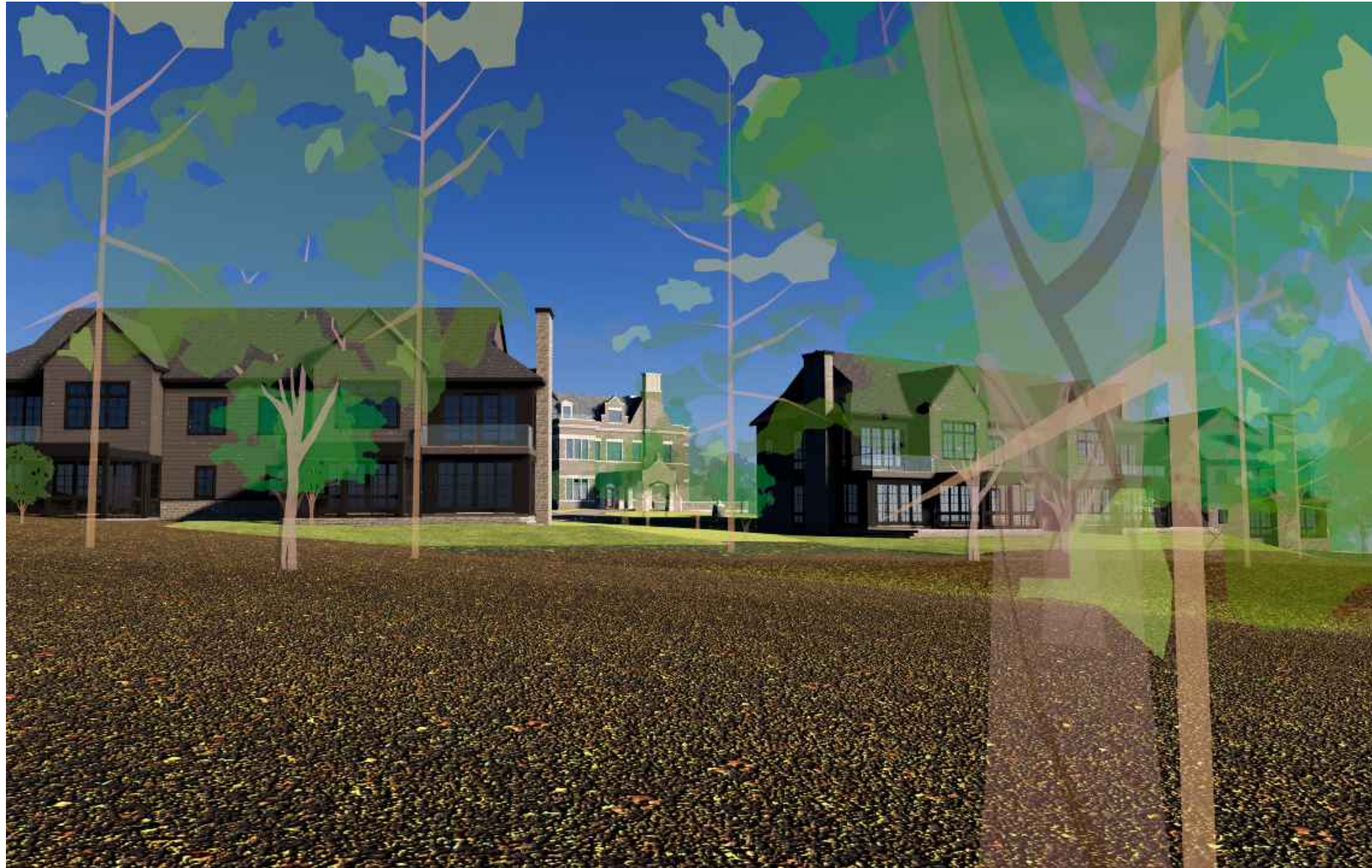
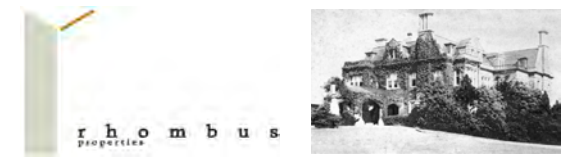
Existing View from Lavender Trail Aligned with North West Property Corner - 02/23/2024





View 5 - From Park Aligned with North West Property Corner - 124'





View 5 - From Park Aligned with North West Property Corner - 124' - without Understory





Existing View from Lavender Trail Aligned with South West Property Corner - 11/29/2022



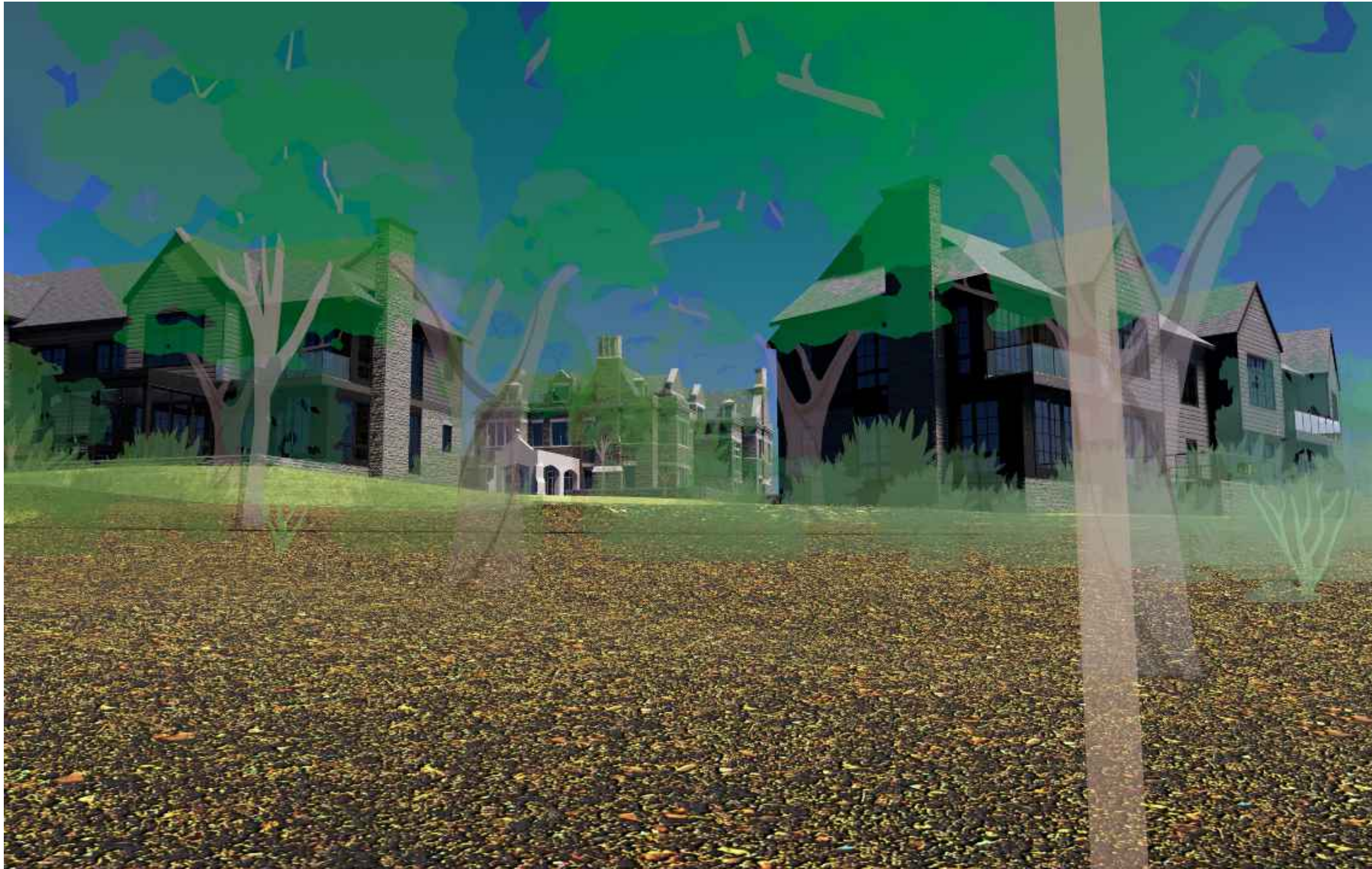
Existing View from Lavender Trail Aligned with South West Property Corner - 11/08/2023



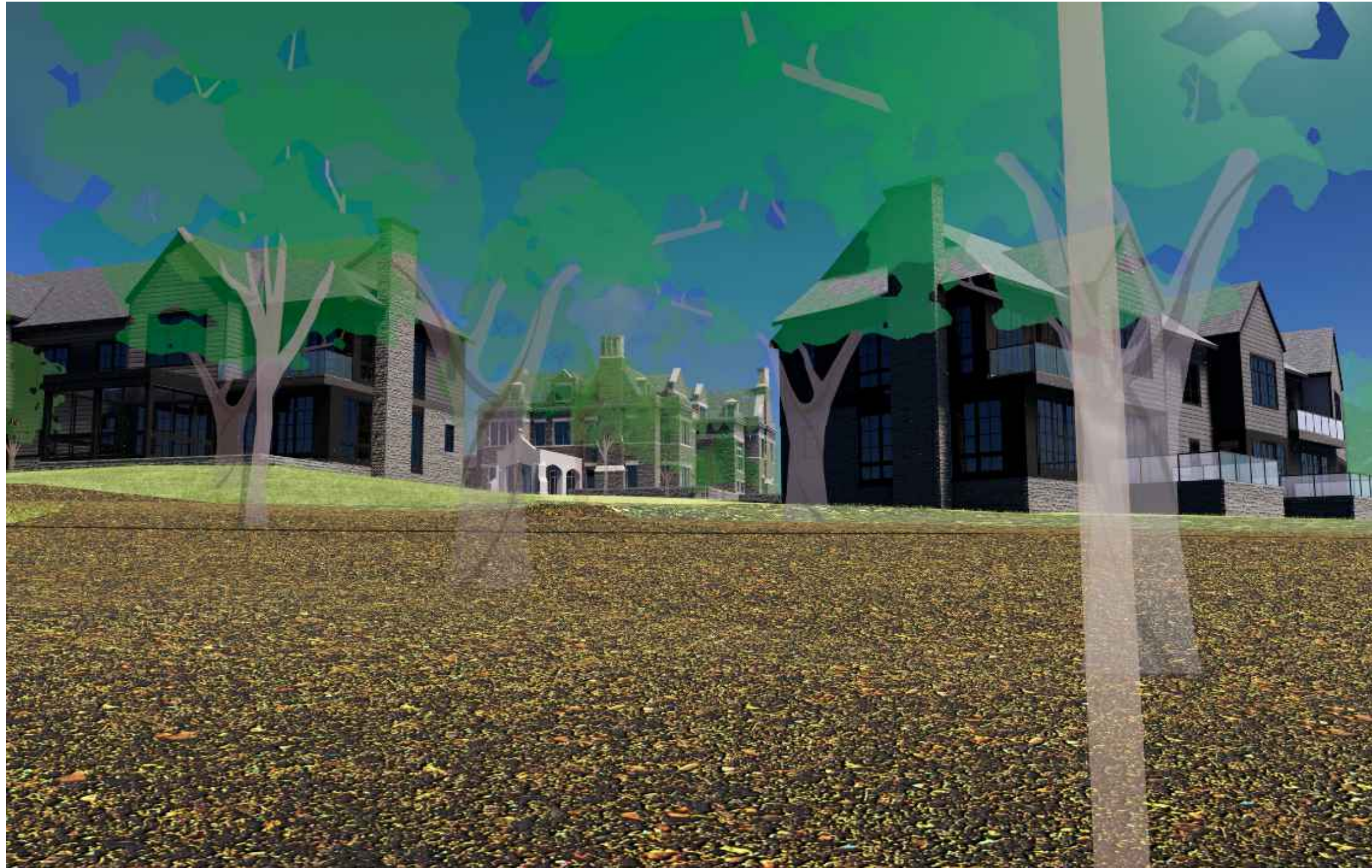
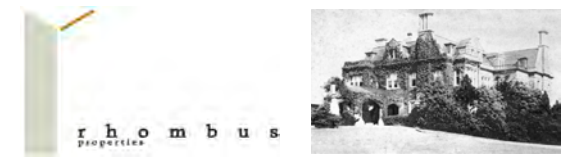


Existing View from Lavender Trail Aligned with South West Property Corner - 02/23/2024





View 7 - From Park Aligned with South West Property Corner - 77'



View 7 - From Park Aligned with South West Property Corner - 77' - without Understory





"THE TRIPLEX" EXTERIOR MATERIALS:



ROOFING SHINGLES-
SMOKEY QUARTZ



SIDING 1-
NIGHT GRAY



WINDOWS & DOORS-
BRONZE



STONE-
SCHIST





"THE TRIPLEX" EXTERIOR MATERIALS:



ROOFING SHINGLES-
SMOKEY QUARTZ



SIDING 1-
NIGHT GRAY



WINDOWS & DOORS-
BRONZE



STONE-
SCHIST





"THE TRIPLEX" EXTERIOR MATERIALS:



ROOFING SHINGLES-
SMOKEY QUARTZ



SIDING 1-
NIGHT GRAY



WINDOWS & DOORS-
BRONZE



STONE-
SCHIST





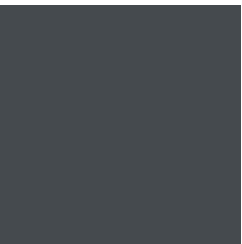
"THE TRIPLEX" EXTERIOR MATERIALS:



ROOFING SHINGLES-
WEATHERED WOOD



SIDING 2-
NIGHT GRAY



WINDOWS & DOORS-
GUNMETAL



STONE-
SCHIST





"THE TWINS" EXTERIOR MATERIALS:



ROOFING SHINGLES-
SMOKEY QUARTZ



SIDING 1-
NIGHT GRAY



WINDOWS & DOORS-
BRONZE



STONE-
SCHIST





"THE TWINS" EXTERIOR MATERIALS:



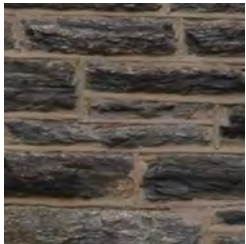
ROOFING SHINGLES-
SMOKEY QUARTZ



SIDING 1-
NIGHT GRAY



WINDOWS & DOORS-
BRONZE



STONE-
SCHIST





View 14 - New Garage Location View from Circle



View 15 - View Between carriage House & Mansion towards West



TRAFFIC PLANNING AND DESIGN, INC.

WWW.TRAFFICPD.COM

November 13, 2023

Mr. Lavi Shenkman
Rhombus Properties
123 Chestnut Street
Philadelphia, PA 19106

RE: Traffic Assessment

209 West Chestnut Hill Avenue (Greylock)
City of Philadelphia, Philadelphia County, PA
TPD No. RHOM.00001

Dear Mr. Shenkman:

Traffic Planning and Design, Inc. (TPD) has completed a traffic assessment for the proposed multi-family development in the City of Philadelphia, Philadelphia County, PA. The site is located on the western side of W. Chestnut Hill Avenue, south of Crefeld Street.

PROJECT DESCRIPTION

The proposed redevelopment of the Greylock site will consist of a total of 15 dwelling units within multifamily buildings and twins. The Proposed Site Plan prepared by Millan Architects, dated 10/17/23, is **attached**.

EXISTING ROAD NETWORK

The existing West Chestnut Hill Avenue roadway characteristics, in the vicinity of the site, are summarized in **Table 1** and below.

TABLE 1 - ROADWAY CHARACTERISTICS

Roadway	Functional Roadway Classification	Directional Orientation	Posted Speed Limit
West Chestnut Hill Avenue	Major Collector	North-South	25 mph

West Chestnut Hills Avenue is a north-south, two-way City street with a cartway width of approximately 26 feet with one vehicular travel lane in each direction, and a shoulder on each side of the road. Slate sidewalks currently accommodate pedestrian traffic along both sides of the road, in the vicinity of the site.

In addition, an inventory of the existing transit facilities in the vicinity of the site was performed and documented. The area is currently serviced by Southeastern Pennsylvania Transportation Authority (SEPTA), including the following:

- SEPTA Regional Rail Chestnut Hill East Station, along the Chestnut Hill East Line, located approximately 0.5 miles from the site. Connecting SEPTA bus service for bus routes L and 94 are provided at the station.

- SEPTA Regional Rail Highland Station, along the Chestnut Hill West Line, located approximately 0.4 miles from the site.

EXISTING TRAFFIC VOLUMES

Existing traffic volumes for the study area were obtained from PennDOT's Traffic Information Repository (TIRe). PennDOT collected traffic data along West Chestnut Hill Avenue, north of Crefeld Street in October 2021. Please note, per PennDOT's 8/25/21 Strike-off Letter, traffic data collected on or after September 7, 2021 shall not require adjustments due to COVID.

Based on the October 2021 traffic volume data (**attached**), the West Chestnut Hill Avenue in the area carries approximately 5,988 vehicles per day (vpd). The roadway carries 492 vehicles per hour (vph) during the weekday morning commuter peak hour, and 623 vph during the weekday evening peak hour.

SIGHT DISTANCE RECOMMENDATIONS

The required sight distances for the existing site driveway, which is proposed to be utilized for the redevelopment are listed below. In general, recommended safe sight distances depend upon the posted speed limit and roadway grades. The required sight distances at the proposed driveways are determined in accordance with PennDOT Publication 282 Highway Occupancy Permit Guidelines and PennDOT's safe stopping sight distance standard, which is calculated by the following equation:

$$SSSD = 1.47VT + V^2/[30(f \pm g)]$$

SSSD = safe stopping sight distance (acceptable sight distance)

V = Vehicle Speed

T = Perception Reaction Time of Driver (2.5 seconds)

f = Coefficient of Friction for Wet Pavements

g = Percent of Roadway Grade Divided by 100

Table 2 shows the estimated required sight distances at the site driveway for vehicles entering and exiting the site.

TABLE 2 - SIGHT DISTANCE REQUIREMENTS

	Direction	Posted Speed Limit	Estimated Roadway Grade	Required Sight Distance (feet)
Exiting Movements	To the left	25 mph	-7%	159
	To the right	25 mph	-10%	166
Entering Left Turns	Approaching same direction	25 mph	-10%	166
	Approaching opposite direction	25 mph	-7%	159

Based on a review of the site plan an online review of images, sufficient sight distance is available to facilitate motorists exiting and exiting the site driveway onto West Chestnut Hill Avenue.

TRIP GENERATION & TRAFFIC VOLUME CONTRIBUTION

The trip generation rates for the proposed site were obtained from the manual *Trip Generation*, Eleventh Edition, 2021, an Institute of Transportation Engineers (ITE) Informational Report. For the project site, Land Use Code #215 (Single-Family Attached Housing) from *Trip Generation* was used to calculate the vehicular trips the development will generate during the studied time periods. Please note, TPD also evaluated other similar multifamily land use for determining the appropriate land use to utilize. Based on this evaluation, the Single-Family Attached Housing land use results in slightly more or the same

number of peak hour trips than the multifamily uses, which results in a conservative (i.e. highest) estimate for this evaluation. **Table 3** shows the trip generation data and directional percentages for studied time periods.

TABLE 3 – TRIP GENERATION DATA

Land Use	Time Period	Trip Generation Rates	Directional Distribution	
			Entering	Exiting
Single-Family Attached Housing (ITE LU#215)	Average Weekday (24-Hours)	$T = 7.62*(X)-50.48$	50%	50%
	Weekday A.M. Peak	$T = 0.48*(X)$	25%	75%
	Weekday P.M. Peak	$T = 0.57*(X)$	59%	41%

T = Total Trips **X** = Independent Variable (dwelling units)

The data presented in the *Trip Generation* manual, as outlined in **Table 3**, is based on data collected in suburban areas throughout the United States; and, therefore, represent typical U.S. suburban conditions. As such, the modal split of alternative, non-vehicular trips such as pedestrian, bicycle, and transit trips, found in more urban-like settings are not specifically addressed with the empirical data contained in the *Trip Generation* manual. The presence of pedestrian, bicycle, and transit facilities near the proposed site, increases the number of non-vehicle trips. In order to provide a conservative estimate (i.e. high), no modal reduction was applied to the vehicular trip generation. **Table 4** summarizes the vehicular / non-vehicular trip generation for the proposed development.

TABLE 4 – TRIP GENERATION

Time Period	Total Vehicle Trips		
	Total	Enter	Exit
Average Weekday (24-Hour Total)	64	32	32
Weekday AM Peak	7	2	5
Weekday PM Peak	9	5	4

Based on the trip generation analysis summarized in **Table 4**, the proposed development is anticipated to generate approximately:

- 64 new vehicular trips during a typical weekday. Please note, one (1) car is equal to two (2) vehicles trips; one entering and one exiting. As such, the proposed development is anticipated to generate 32 cars over a typical weekday.
- 7 new vehicular trips during the weekday A.M. peak hour, or 1 new trip every 8-9 minutes during the peak hour.
- 9 new vehicular trips during the weekday P.M. peak hour, or 1 new trip every 6-7 minutes during the peak hour.

Table 5 compares the anticipated site trip generation to the exiting traffic volumes along West Chestnut Hill Avenue.

TABLE 5 – TRAFFIC VOLUME CONTRIBUTION

Time Period	West Chestnut Hill Avenue			
	Exiting Traffic Volume	Proposed Development Traffic	Total	% Increase
Average Weekday (24-Hour Total)	5988	64	6052	1.1%
Weekday AM Peak	492	7	499	1.4%
Weekday PM Peak	623	9	632	1.4%

As shown in **Table 5**, the proposed development traffic is anticipated to contribute less than 1.5% of the total traffic volume along West Chestnut Hill Avenue. As such, it is TPD's opinion that the proposed redevelopment will result in no discernable difference in traffic operations to the motoring public.

If there are questions or additional information is required relative to this analysis, please call anytime.

Sincerely,

TRAFFIC PLANNING AND DESIGN, INC.



Guido W. DiMartino, P.E.

Regional Leader – Transportation Planning

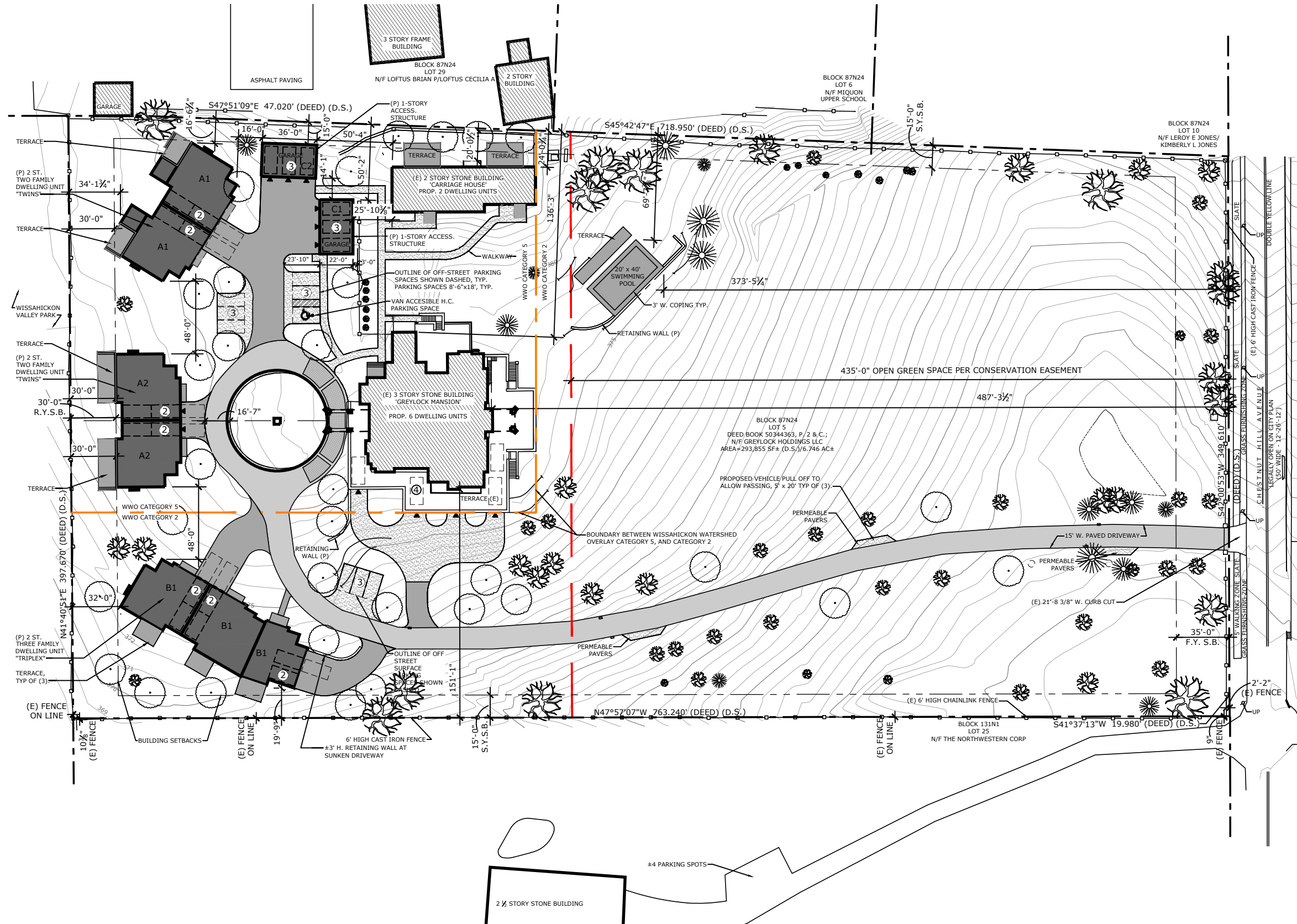
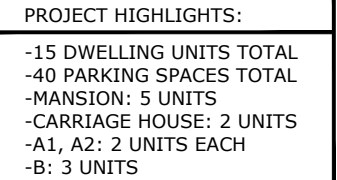
Enclosures: Proposed Site Plan
 TIRE Data



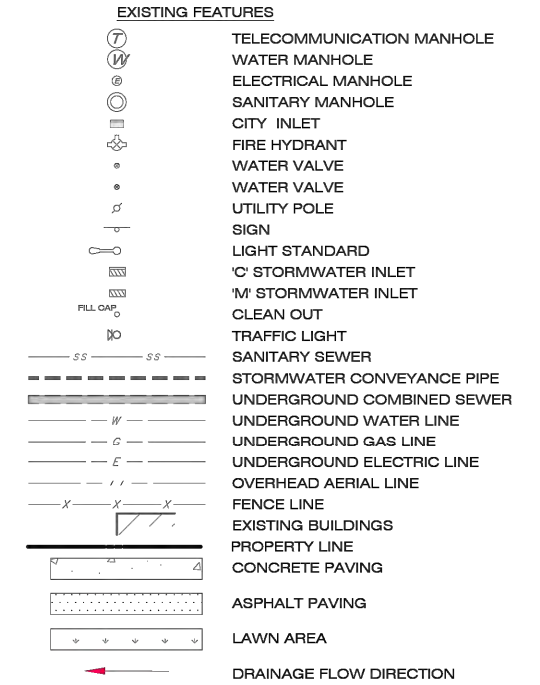
Greylock Estate - 209 W. Chestnut Hill Ave									
Total Impervious Coverage for Total Lot Area									
Total Lot Area (SF.)	295,315								
	11/2/2023			2/19/2024			3/7/2024		
	Existing (SF.)	Proposed (SF.)	Total (SF.)	Existing (SF.)	Proposed (SF.)	Total (SF.)	Existing (SF.)	Proposed (SF.)	Total (SF.)
Building	9,125	14,977	24,102	9,125	13,251	22,376	9,125	13,403	22,528
Others (Driveway, terraces, walkways)	31,787	7,786	39,573	31,787	9,070	40,857	31,787	9,553	41,339
Total Imp. Coverage	40,912	22,763	63,675	40,912	22,321	63,233	40,912	22,956	63,867
Building Gross Area									
	11/2/2023			2/19/2024			3/7/2024		
	Existing (SF.)	Proposed (SF.)	Total (SF.)	Existing (SF.)	Proposed (SF.)	Total (SF.)	Existing (SF.)	Proposed (SF.)	Total (SF.)
Existing (Renovation)	28,891	-327	28,564	28,891	1,540	30,431	28,891	-327	28,564
Mansion	23,593	-327	23,266	23,593	1,540	25,133	23,593	-327	23,266
Carriage House	5,298	0	5,298	5,298	0	5,298	5,298	0	5,298
New Construction	0	30,451	30,451	0	24,747	24,747	0	14,346	14,346
Terraces (Excl. Garage)	0	15,819	15,819	0	0	0	0	0	0
Twins (Excl. Basement)	0	14,632	14,632	0	14,346	14,346	0	14,346	14,346
Triplex (Inc. Garage, Excl. Bsmt)	0	0	0	0	10,401	10,401	0	10,724	10,724
Accessory Structures	0	1,824	1,824	0	1,584	1,584	0	1,584	1,584
Pool House	0	240	240	0	0	0	0	0	0
Garage	0	1,584	1,584	0	1,584	1,584	0	1,584	1,584
Total Gross	28,891	31,948	60,839	28,891	27,871	56,762	28,891	15,603	44,494
Total Parking Spaces on Site									
	11/2/2023			2/19/2024			3/7/2024		
	Existing	Proposed	Total	Existing	Proposed	Total	Existing	Proposed	Total
Parking Spaces	10	24	34	10	29	39	10	30	40

Impervious Coverage Calculations for Portion of Lot within WWOD Category 2





- Preserve and restore existing historic Greylock Mansion and Carriage House, terraces, and driveway.
- Restore and improve the site and landscape, including a native species meadow and shade trees.
- Retain the open green space in the front yard.
- Restore public views of the Greylock Mansion and Carriage House from W Chestnut Hill Ave.
- Restore sidewalk and iron estate fencing along W Chestnut Hill Ave.
- Provide stormwater management that controls and improves the runoff conditions along the Wissahickon Park and adjacent property lines.
- Development plan that ensures long term health and financial viability of property, and operations and maintenance of the existing historical buildings, terrace, driveway in the future.
- Create construction jobs during the project.
- Add real estate value and taxes to the city.
- Add neighbors to community and patrons to local businesses.



THE PROPERTY AT 209 W. CHESTNUT HILL AVENUE CONTAINS 293,855sf OR 6.746acres. THE PROPERTY IS MADE UP OF 3 MAIN DRAINAGE AREAS WHICH DISCHARGE TO DIFFERENT AREAS OF THE SITE. THE UPPER AREA OF THE SITE OR DRAINAGE AREA #1 IS MADE UP OF AN ASPHALT DRIVEWAY AND PARKING AREA, MAINTAINED LAWN AREA, AND TREE CANOPY COVER. THIS DRAINAGE DRAINS VIA OVERLAND FLOW TO THE REAR OF THE PROPERTY BORDERING THE WISSAHICKON VALLEY AND SECTIONS OF THE LAVENDER TRAIL. DRAINAGE AREA #2 DISCHARGES TO THE SOUTHERN PROPERTY LINE BORDERING NEIGHBORING PARCEL AT 305 W. CHESTNUT HILL AVE. THIS DRAINAGE AREAS CONTAINS A MAJORITY OF THE ASPHALT ENTRY DRIVEWAY, THE MAIN BUILDING AND TERRACE, MAINTAINED LAWN AREA, AND TREE CANOPY COVER. DRAINAGE AREA #3 IS THE LARGEST DRAINAGE AREA ON SITE. THE MAJORITY OF THE OPEN SPACE IS AN OVERGROWN SUCCESSIVE LAWN AREA WITH MOWN PATHWAYS. THE REST OF THIS DRAINAGE AREA IS TREE CANOPY COVER AND MINIMAL IMPERVIOUS COVER FROM THE ACCESSORY BUILDING. DRAINAGE AREA #3 DISCHARGES VIA OVERLAND FLOW TO THE RIGHT-OF-WAY IN W. CHESTNUT HILL AVENUE WHERE THERE ARE TWO CITY OWNED STORMWATER INLETS.

02/19/2024



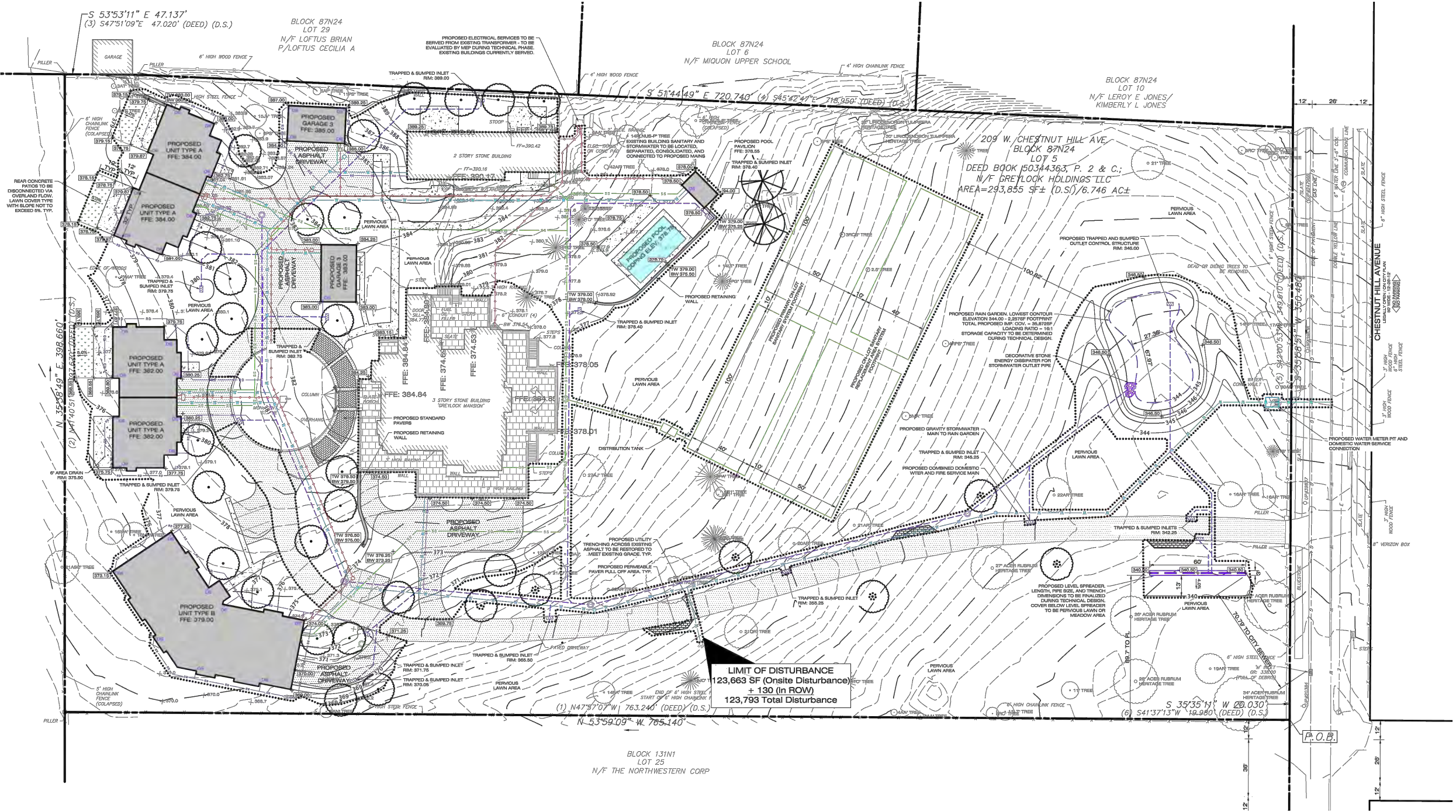
LEGEND

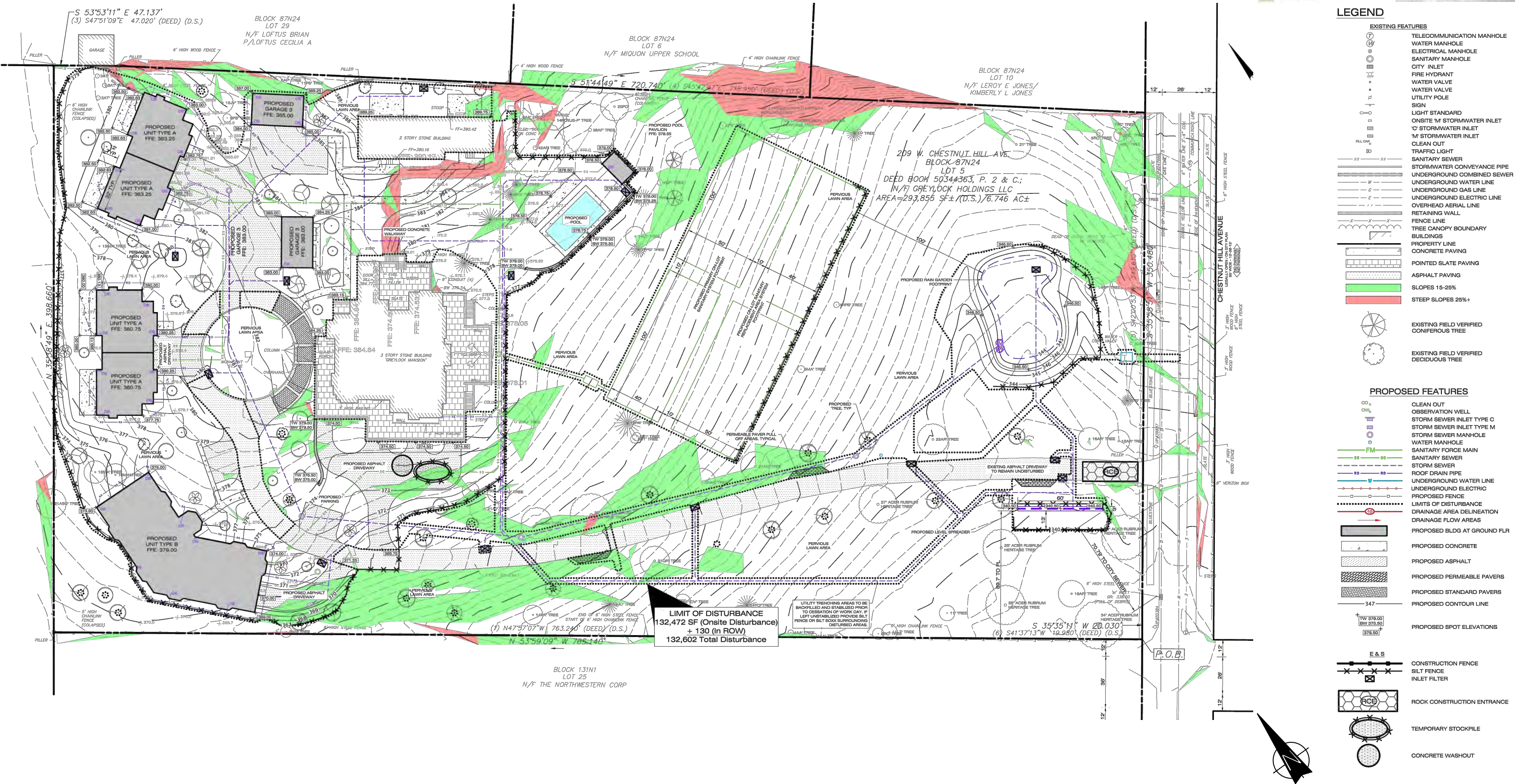
EXISTING FEATURES

- TELECOMMUNICATION MANHOLE
- WATER MANHOLE
- ELECTRICAL MANHOLE
- SANITARY MANHOLE
- CITY INLET
- FIRE HYDRANT
- WATER VALVE
- WATER VALVE
- UTILITY POLE
- SIGN
- LIGHT STANDARD
- ONSITE 'M' STORMWATER INLET
- 'C' STORMWATER INLET
- 'M' STORMWATER INLET
- CLEAN OUT
- TRAFFIC LIGHT
- SANITARY SEWER
- STORMWATER CONVEYANCE PIPE
- UNDERGROUND COMBINED SEWER
- UNDERGROUND WATER LINE
- UNDERGROUND GAS LINE
- UNDERGROUND ELECTRIC LINE
- OVERHEAD AERIAL LINE
- RETAINING WALL
- FENCE LINE
- TREE CANOPY BOUNDARY
- BUILDINGS
- PROPERTY LINE
- CONCRETE PAVING
- POINTED SLATE PAVING
- ASPHALT PAVING
- EXISTING FIELD VERIFIED CONIFEROUS TREE
- EXISTING FIELD VERIFIED DECIDUOUS TREE

PROPOSED FEATURES

- CLEAN OUT
- OBSERVATION WELL
- STORM SEWER INLET TYPE C
- STORM SEWER INLET TYPE M
- STORM SEWER MANHOLE
- WATER MANHOLE
- SANITARY FORCE MAIN
- SANITARY SEWER
- STORM SEWER
- ROOF DRAIN PIPE
- UNDERGROUND WATER LINE
- UNDERGROUND ELECTRIC
- PROPOSED FENCE
- LIMITS OF DISTURBANCE
- DRAINAGE AREA DELINEATION
- DRAINAGE FLOW AREAS
- PROPOSED BLDG AT GROUND FLR
- PROPOSED CONCRETE
- PROPOSED ASPHALT
- PROPOSED PERMEABLE PAVERS
- PROPOSED STANDARD PAVERS
- PROPOSED CONTOUR LINE

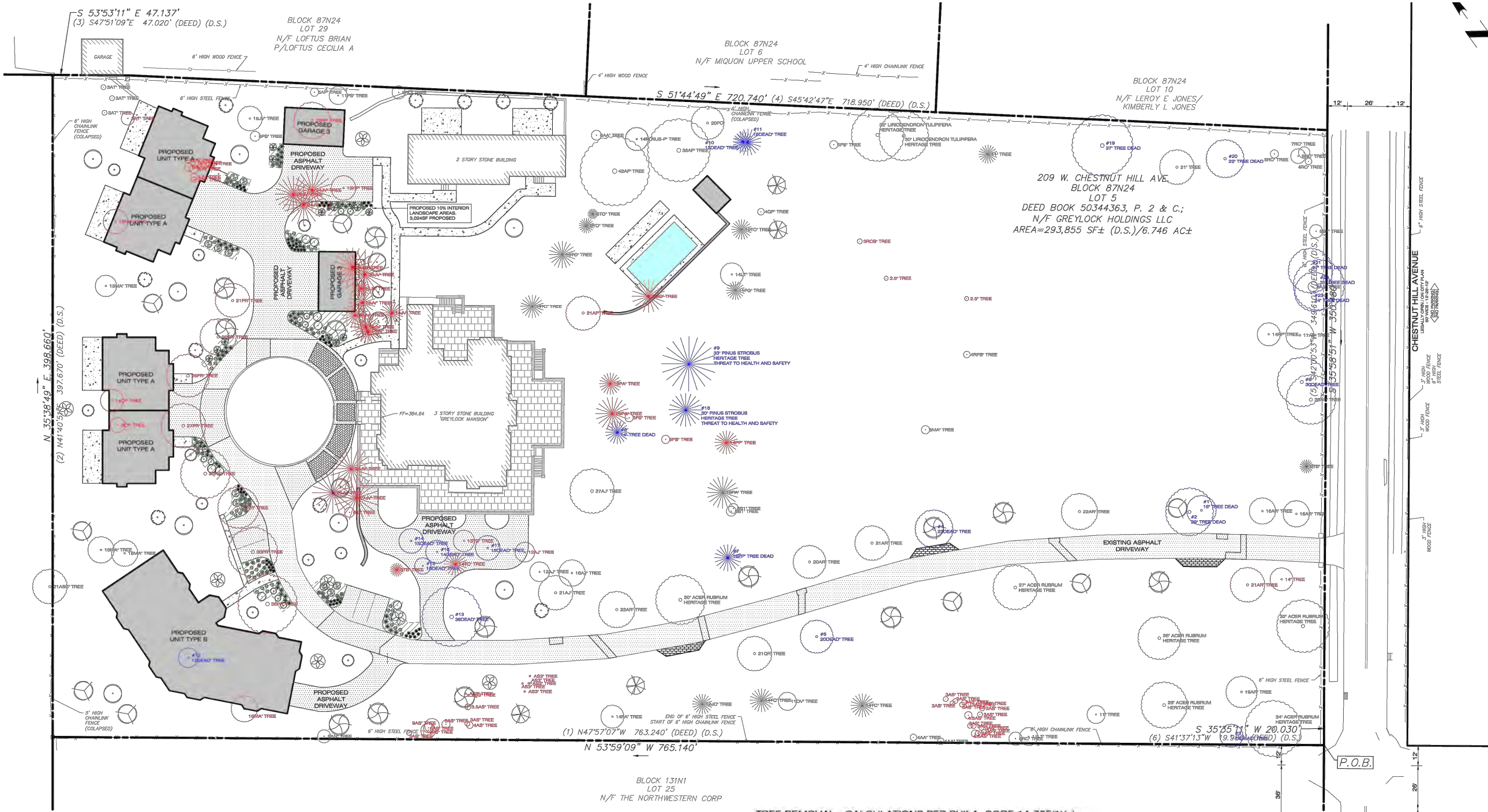




LEGEND	
EXISTING FEATURES	
	TELECOMMUNICATION MANHOLE
	WATER MANHOLE
	ELECTRICAL MANHOLE
	SANITARY MANHOLE
	CITY INLET
	FIRE HYDRANT
	WATER VALVE
	WATER VALVE
	UTILITY POLE
	SIGN
	LIGHT STANDARD
	ONSITE "M" STORMWATER INLET
	"C" STORMWATER INLET
	"M" STORMWATER INLET
	CLEAN OUT
	TRAFFIC LIGHT
	SANITARY SEWER
	STORMWATER CONVEYANCE PIPE
	UNDERGROUND COMBINED SEWER
	UNDERGROUND WATER LINE
	UNDERGROUND GAS LINE
	UNDERGROUND ELECTRIC LINE
	OVERHEAD AERIAL LINE
	RETAINING WALL
	FENCE LINE
	TREE CANOPY BOUNDARY
	BUILDINGS
	PROPERTY LINE
	CONCRETE PAVING
	POINTED SLATE PAVING
	ASPHALT PAVING
	SLOPES 15-25%
	STEEP SLOPES 25%+
	EXISTING FIELD VERIFIED CONIFEROUS TREE
	EXISTING FIELD VERIFIED DECIDUOUS TREE
PROPOSED FEATURES	
	CLEAN OUT
	OBSERVATION WELL
	STORM SEWER INLET TYPE C
	STORM SEWER INLET TYPE M
	STORM SEWER MANHOLE
	WATER MANHOLE
	SANITARY FORCE MAIN
	SANITARY SEWER
	STORM SEWER
	ROOF DRAIN PIPE
	UNDERGROUND WATER LINE
	UNDERGROUND ELECTRIC
	PROPOSED FENCE
	LIMITS OF DISTURBANCE
	DRAINAGE AREA DELINEATION
	DRAINAGE FLOW AREAS
	PROPOSED BLDG AT GROUND FLR
	PROPOSED CONCRETE
	PROPOSED ASPHALT
	PROPOSED PERMEABLE PAVERS
	PROPOSED STANDARD PAVERS
	PROPOSED CONTOUR LINE
	PROPOSED SPOT ELEVATIONS
	E & S
	CONSTRUCTION FENCE
	SILT FENCE
	INLET FILTER
	ROCK CONSTRUCTION ENTRANCE
	TEMPORARY STOCKPILE
	CONCRETE WASHOUT



Existing Tree Conditions



LEGEND

EXISTING FEATURES

- TELECOMMUNICATION MANHOLE
- WATER MANHOLE
- ELECTRICAL MANHOLE
- SANITARY MANHOLE
- CITY INLET
- FIRE HYDRANT
- WATER VALVE
- UTILITY POLE
- SIGN
- LIGHT STANDARD
- ON-SITE 'M' STORMWATER INLET
- 'C' STORMWATER INLET
- 'M' STORMWATER INLET
- CLEAN CUT
- TRAFFIC LIGHT
- SANITARY SEWER
- STORMWATER CONVEYANCE PIPE
- UNDERGROUND COMBINED SEWER
- UNDERGROUND WATER LINE
- UNDERGROUND GAS LINE
- UNDERGROUND ELECTRIC LINE
- OVERHEAD AERIAL LINE
- RETAINING WALL
- FENCE LINE
- TREE CANOPY BOUNDARY
- BUILDINGS
- PROPERTY LINE
- CONCRETE PAVING
- POINTED SLATE PAVING
- ASPHALT PAVING
- 10% INTERIOR LANDSCAPING

EXISTING TREE TO REMAIN

EXISTING TREE TBR

EXEMPT DEAD OR DYING TREE TBR

PLANT SCHEDULE

QUANTITY	CODE	BOTANICAL NAME	COMMON NAME	NOTES
4	Ps	Pinus strobus	White Pine	3" Cal. B&B
8	La	Liquidambar styraciflua	Sweetgum	3" Cal. B&B
1	Bn	Betula nigra	River Birch	Multi stem B&B
13	Gt	Gleditsia triacanthus	Honey Locust	3" Cal. B&B
12	Cc	Cercis canadensis	Red Bud	3" Cal. B&B
10	Ag	Acer griseum	Paperbark Maple	3" Cal. B&B
7	Am	Amelanchier canadensis	Service Berry	Multi stem B&B
SHRUBS				
11	Iv	Ites virginica	Virginia Sweetshrub	#3 cont.
8	Vd	Viburnum dentatum	Arrowwood	#3 cont.
10	Ca	Clethra alnifolia	Sweet Pepperbush	#3 cont.
12	CS	Cornus sericea	Red Osier Dogwood	#3 cont.
PERENNIALS				
59	Pv	Panicum virgatum	Red Switch Grass	#1 cont.
57	Ss	Schizachyrium scoparium	Little Bluestem	#1 cont.
50	PC	Echinacea purpurea	Purple Coneflower	#1 cont.
60	Ah	Artemisia ludoviciana	Blue Star	#1 cont.

CREDITS FOR PRESERVING EXISTING TREES - CALCULATIONS PER PHILA. CODE 14-705(1)(h)

TOTAL EXISTING TREES GREATER THAN 12" TO BE PRESERVED = 42
- (12" x 42) = 504"

TOTAL EXISTING 8"-12" TREES TO BE PRESERVED = 15
- (8" x 15) = 120"

TOTAL EXISTING 5"-7" TREES TO BE PRESERVED = 10
- (5" x 10) = 50"

TOTAL PRESERVATION OF EXISTING TREE CREDITS = 674"

*HEALTHY TREES AS PART OF A DEVELOPMENT PROJECT MAY OBTAIN CREDITS TOWARD TREES REQUIRED BY THE CITY OF PHILADELPHIA ZONING CODE, OTHER THAN THOSE REQUIRED UNDER 14-705(1)(g).

TREE REMOVAL - CALCULATIONS PER PHILA. CODE 14-705(1)(g)

TOTAL EXISTING HEALTHY TREE CALIPER INCHES ON SITE - 2,155.5"
TOTAL EXISTING EXEMPT DEAD OR DYING TREE CALIPER INCHES ON SITE - 505"
TOTAL EXISTING HEALTHY TREE CALIPER INCHES ON SITE TO BE REMOVED - 957.5"

PROPOSED TREE PLANTINGS

TOTAL PROPOSED MIN. 3" CALIPER TREES TO BE PLANTED - 65
- (65 x 3") = 195"
TOTAL PROPOSED TREE CALIPER INCHES TO BE REPLACED = 195"

TREE REPLACEMENT REQUIREMENT - CALCULATIONS PER PHILA. CODE 14-705(1)(g)

TOTAL REQUIRED TREE CALIPER INCHES CALCULATION
- (957.5" - 195") = 762.5"
TOTAL TREE CALIPER INCHES REQUIRED TO BE REPLACED = 762.5"

PLANTING PLAN WITH TYPOLOGIES



- LEGEND
- LAWN
 - GARDENESQUE
 - WOODLAND BUFFER
 - MANAGED MEADOW
 - WET MEADOW



GARDENESQUE REFERENCE IMAGES



WOODLAND BUFFER REFERENCE IMAGES



MANAGED MEADOW REFERENCE IMAGES



managed for invasives



winter as time goes by

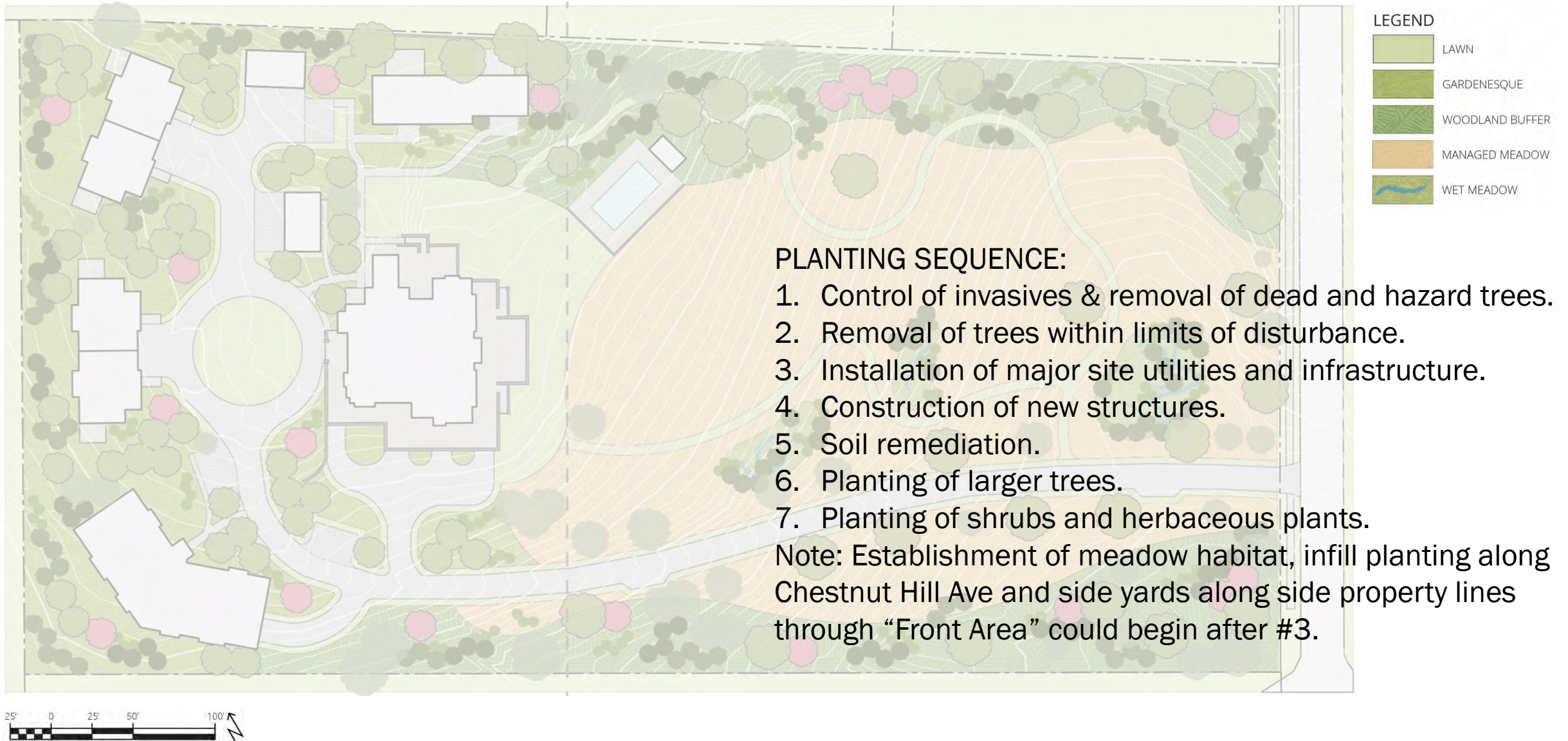


stylized

WET MEADOW – REFERENCE IMAGES



PLANTING PLAN WITH TYPOLOGIES





EXISTING CONTEXT PHOTO:NORTH WEST ELEVATION



EXISTING CONTEXT PHOTO:SOUTH WEST ELEVATION



EXISTING CONTEXT PHOTO:SOUTH WEST ELEVATION



EXISTING CONTEXT PHOTO:SOUTH EAST ELEVATION

GREYLOCK
209 W Chestnut Hill Avenue, Philadelphia, PA 19118



EXISTING CONTEXT PHOTO:
SIDE ENTRY



EXISTING CONTEXT PHOTO: TYPICAL WINDOW BAY
AT SOUTH EAST ELEVATION



EXISTING CONTEXT PHOTO:
SIDE ENTRY



EXISTING CONTEXT PHOTO: ROOF DECK AT NORTH EAST ELEVATION

Existing Mansion Photos



EXISTING CONTEXT PHOTO



EXISTING CONTEXT PHOTO: NORTH EAST ELEVATION



EXISTING CONTEXT PHOTO: NORTH WEST ELEVATION



EXISTING CONTEXT PHOTO: SOUTHWEST ELEVATION



EXISTING CONTEXT PHOTO: SOUTH EAST ELEVATION